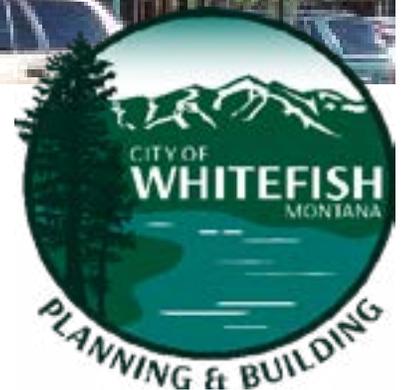
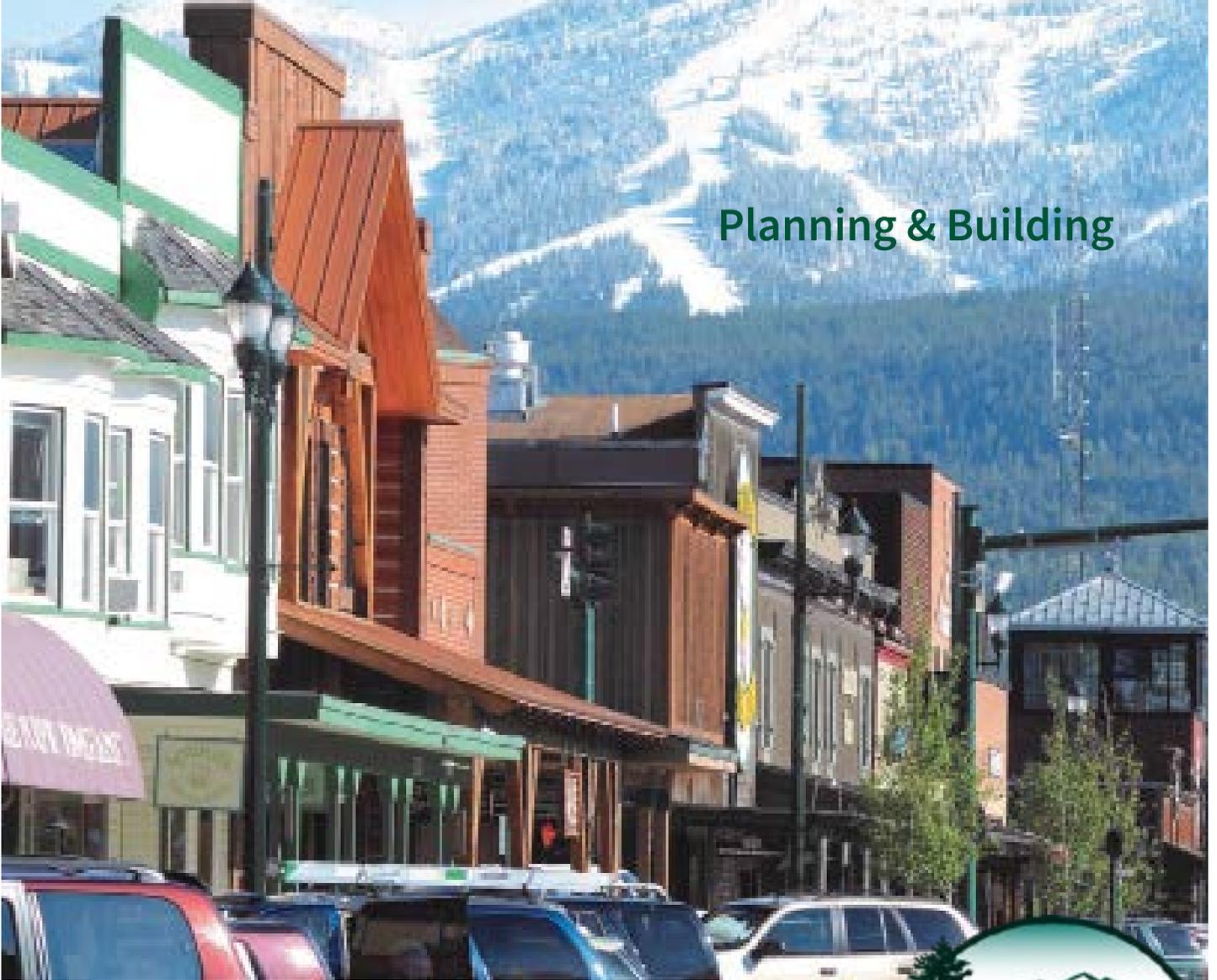


Architectural Review Standards

Planning & Building



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Original Adoption: Ordinance 03-10, 4-21-03
 Amended: Ord. 04-03, 3-1-04; Ord. 06-02, 2-6-06, Ord. 06-28, 10-2-06; Ord 07-11, 5-7-07; Ord. 07-12, 5-7-07; Ord 08-23, 11-17-08; Ord 09-13; 8-17-09; Ord 20-15, 11-2-20; Ord 23-14, 8-21-23

VISION STATEMENT

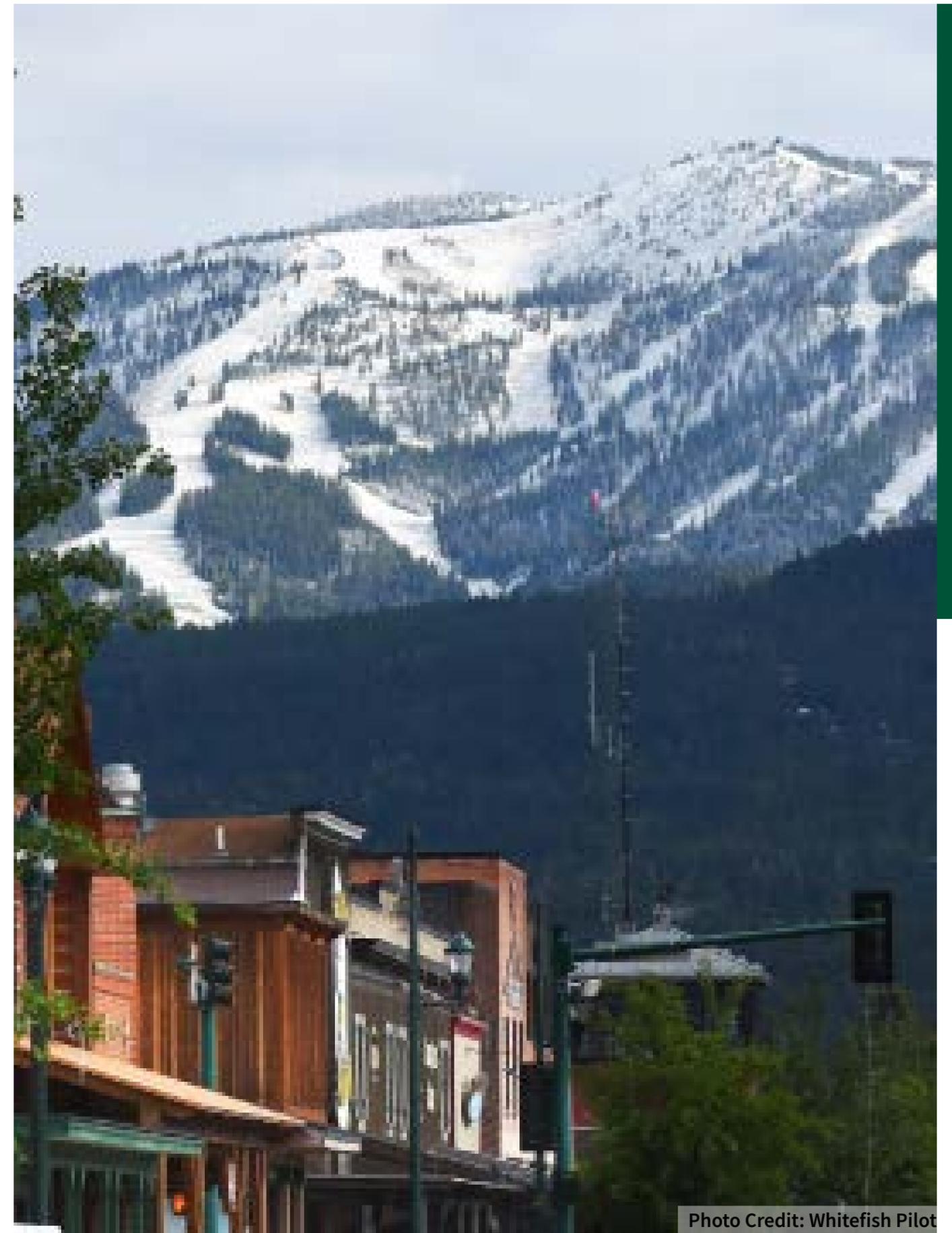
The City of Whitefish is a unique and wonderful place. Beginning as a blue-collar railroad and logging town, it has achieved an additional identity as a resort destination with world-class skiing, fishing, golf, and proximity to Glacier National Park. It is a town with a rich character and heritage in its historic buildings, neighborhoods, its people and stories.

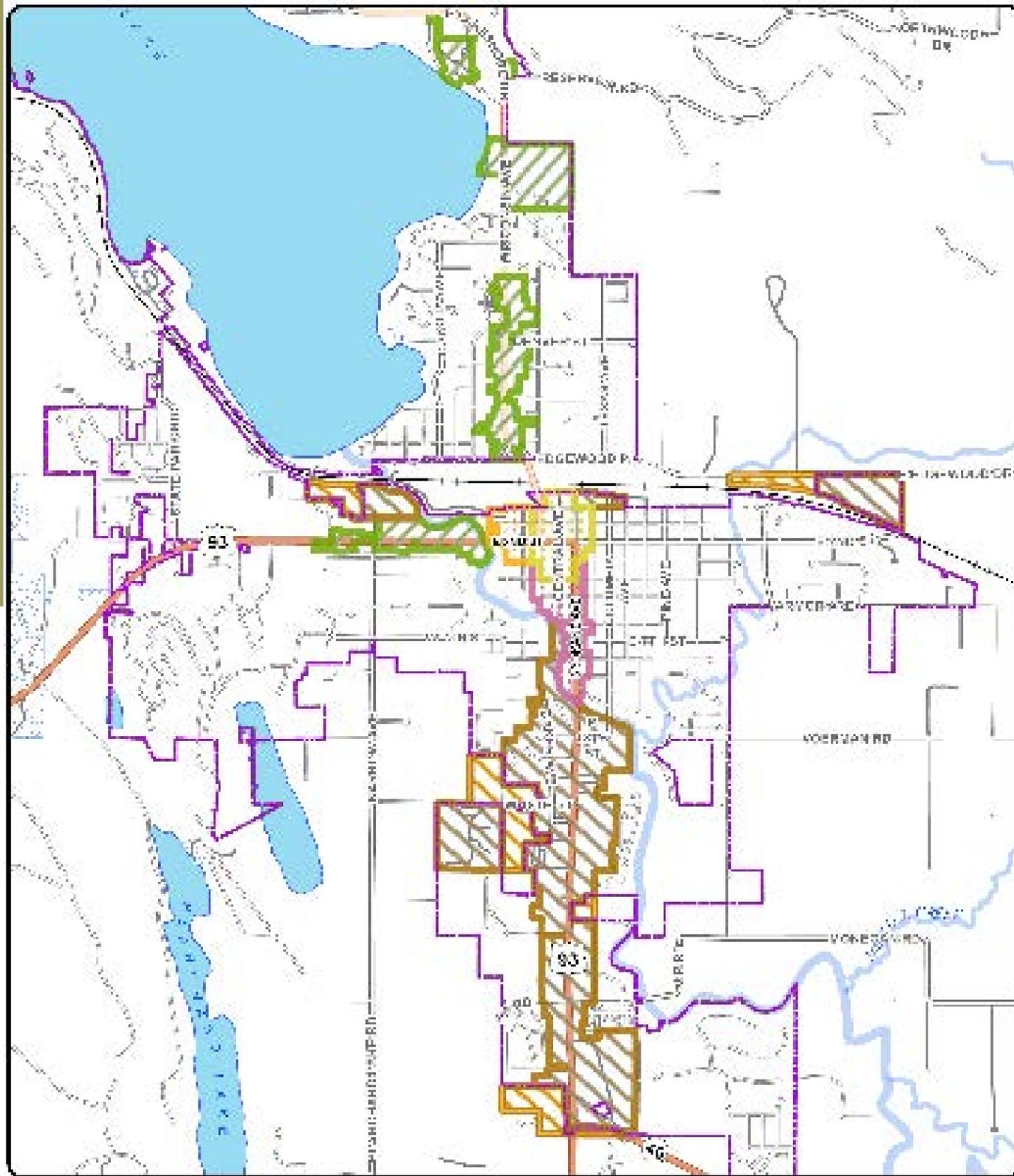
Moving into the future, we intend to continue developing a community that encourages interaction, involvement and vitality among its people. To maintain Whitefish's character, and to respond to the current challenges of rapid

growth, we must focus our efforts toward good planning and design.

Architecture should be a response to the site and environment as much as an organization of program. Honesty of form, material selection and application, and structure without adding unnecessary clutter or faux components contributes to architecture that is authentic, flexible, and reflective of Whitefish.

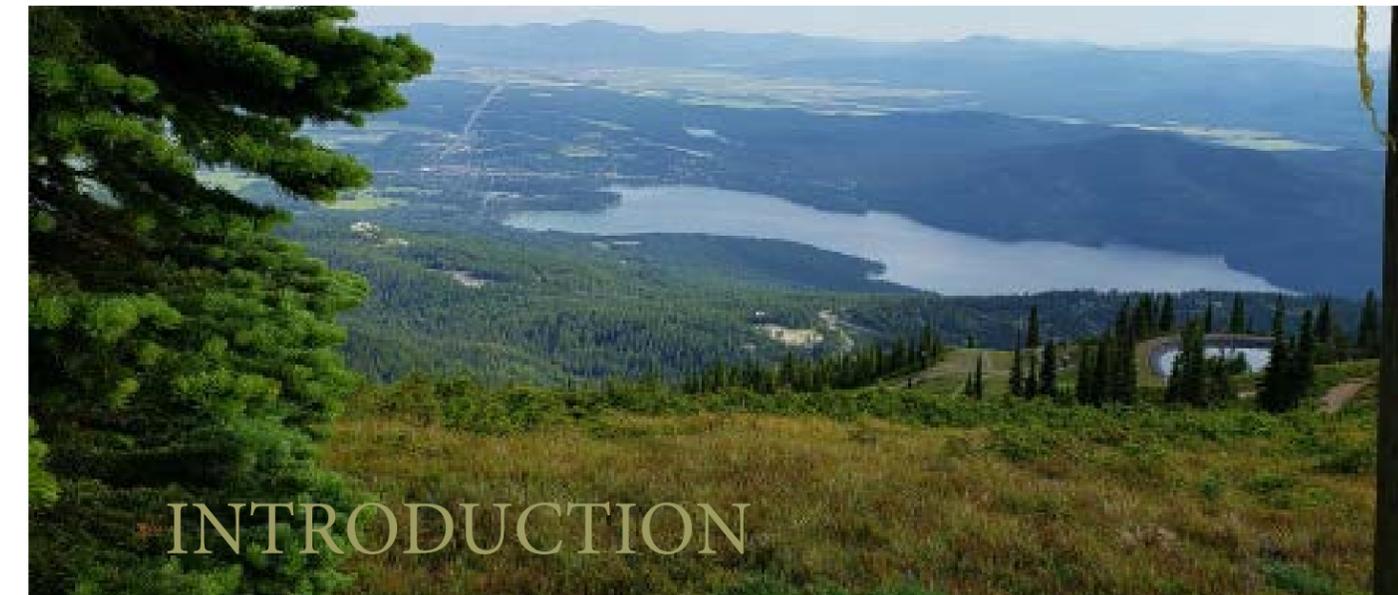
This will result in a community that preserves its diversity, welcomes visitors, and respects the physical environment that is so important to those who love Whitefish.





Architectural Design Districts

- | | | | | | |
|--|---------------------------|--|---------------------------|--|-----------------------|
| | Old Town Central District | | Highway District | | Whitefish City Limits |
| | Old Town North District | | Highway District County | | |
| | Old Town South District | | Resort Community Business | | |



Chapter 1

Why Have Design Standards?

This document presents Architectural Design Standards for building in Whitefish. They are based on a commitment to preserving the City's historic character and heritage through high quality design of new buildings.

The purpose of this document is to inform property owners about the City's design policies. Understanding these policies will help owners in making decisions about their buildings by focusing on the principles of urban design, which promote an environment scaled to pedestrians, while maintaining cohesive neighborhood identity and respecting the unique qualities of Whitefish.

Topics Covered:

- Application and Review Process
- Site Planning
- Landscaping
- Parking
- Lighting Design
- Required Architectural Building Design Elements

Design Districts:

The Architectural Review Standards divide the Whitefish Planning Jurisdiction into four different Design Districts, which are identified by the unique characteristics found within each district. The boundaries of the districts are either streets or natural features, such as the Whitefish River or Whitefish Lake.

- Highway District (Highway 93 South, Highway 40, and areas zoned Industrial)
- Old Town Districts (Central, South, and Railway)
- Resort Community Business District (Wisconsin Avenue and Highway 93 North)
- Residential District (except detached single family and duplexes)

Public/Institutional facilities are located in all design districts. New public facilities are expected to fit into the design district where they are located.



APPLICATION & REVIEW PROCESS

Chapter 2

The Architectural Design Standards are adopted by ordinance and have the same force of law as the City of Whitefish Zoning Regulations. The Standards contain both broader objectives and more specific architectural guidelines than the zoning regulations.

Although the Standards are more than merely advisory, they also allow room for consideration of design proposals not found within the Standards. Each project is different. Therefore, some standards might be emphasized while others are de-emphasized, but no standard will be overlooked.

The objective of the Standards is to encourage quality building and development to enhance both the natural and developed environments in Whitefish.

2.1 Become Familiar with City of Whitefish Regulations and Other Plans

In addition to these Standards, other ordinances and plans adopted by the City of Whitefish may also influence the design and approval of your project. Carefully review all available written materials before starting the planning process.

The other regulations and plans to review are:

- City of Whitefish Zoning Regulations:
 - Sign Regulations
 - Landscaping Regulations
 - Parking Standards
 - Outdoor Lighting Standards
 - Lake and Lakeshore Protection Regulations
- Whitefish City-County Growth Policy, as amended

2.2 Architectural Review Committee

The Architectural Review Committee (“the Committee”) consists of seven members. The specific make-up of the Architectural Review Committee is provided for in §2-10-3A, WCC.

2.3 Scope of Review

The Standards apply to construction of all new buildings:

- Commercial
- Industrial
- Residential (except detached single family and duplexes)
- Public
- Accessory Buildings for Commercial, Industrial, Multi-Family, or Public Buildings (e.g., vehicle canopies, storage sheds, structures for housing equipment, trash enclosures, and open shelters)
- Addition or Exterior Modifications or Alterations to Commercial, Industrial, Multi-Family, or Public Buildings
- Facade Changes or Repainting of Commercial, Industrial, Multi-Family, or Public Buildings

The Zoning Administrator or designee may waive this requirement if it is determined the modification is insignificant and inconsequential to the purpose and intent of the Architectural Review Standards. Finally, any resumption of use after abandonment will require compliance with the Standards. (§11-7-9B, WCC)

In cases where the City Council has reviewed a building design through a land use permit, City Council approval is not a final architectural review approval but an approval of the conceptual plans. Final architectural review is required prior to submitting a building permit application.

2.4 How These Standards Are Organized

The Standards are organized by Design District, as described in Chapter 1. Within each chapter, one will find standards covering a broad range of topics from site design, which includes the site, landscaping, parking and outdoor lighting, to building design, which covers scale, relationship to neighborhood, shape and form, and exterior materials and colors. In addition, there are standards for buildings greater than 10,000 square feet in the Highway District and formula retail standards.

Each chapter will start with a description of the Design District, its location and character. A goal for the District’s evolution will be provided, followed by overarching principals and more specific standards. In addition, photos and/or drawings will be provided as examples of how the various standards can be met.

Terms considered to be mandatory, including but not limited to “will” and “must,” when used in connection with design elements, may at the discretion of the Architectural Review Committee and staff be interpreted as guidelines, if the unique features of a particular application render it in the best interest of the City of Whitefish to do so. In such a case, when granting approval of an application, the Committee must adopt specific recommended findings for staff to consider justifying such interpretation.

2.5 Utilization of Design Professionals

Property owners are strongly encouraged to engage licensed professionals such as architects, engineers, and landscape architects at the beginning of the review process and encourage their participation during Committee meetings. Design professionals can facilitate an efficient review process and often will save the owner time and money. These Standards, the Committee and/or City Planning staff are not intended to take the place of professional assistance.

The Whitefish Building Department requires certain projects to have an architect’s stamp. These projects include commercial, industrial and multi-family (4-plex and up). An architect must be involved in the Architectural Review

process for these types of projects.

2.6 Submittal Deadline for Formal Application or Pre-Application Meeting

The applicant must submit a completed application packet to the Whitefish Planning & Building Department no less than 14 days prior to the regularly scheduled meeting of the Committee. The applicant must submit one copy of the application and supporting materials and an electronic file of the entire application. Material samples may be brought to the meeting.

2.7 Project Review

2.7.1 Informal Advice and Direction

An individual considering a development may consult with City planning staff for informal advice and direction before meeting with the Architectural Review Committee. Such discussion is treated as **advisory only** and any advice given is intended as guidance and is not binding until the Committee has offered a recommendation to city staff. City staff may perform an informal review if an individual submits materials such as plans, and description of the project. The more detail provided about the project the more guidance can be given.

2.7.2 Pre-Application Meeting

City staff and the Committee highly recommend proponents of commercial, industrial, public and multi-family, including fourplex and larger attend a pre-application meeting with the Committee prior to a formal application. Other projects have the option of attending a pre-application meeting or, if planning staff considers the project significant, may require a pre-application. The Committee will not offer a recommendation at a pre-application meeting, but will discuss broad issues such as mass, scale, size, height and neighborhood context.

Conceptual building elevations, along with an application, must be submitted. Other supporting materials for the application are not required for a pre-application meeting. Depending upon the complexity of the project, the Committee may follow-up with written comments.

2.7.3 Formal Application

A formal application consists of a completed

Principles of good design are the cornerstone to a vibrant, functional, and pleasing community.

Architectural Review Application along with the supporting materials and associated fee. Incomplete applications will not be reviewed until the list of items below is provided. A minimum scale of one-eighth inch for plans and elevations is required.

Supporting materials include:

- Site plan including pedestrian, bicycle and vehicular access
- Vicinity map
- Landscaping plan
- Signage concept plan and materials
- Parking plan
- Elevations including the existing building if a remodel or addition is proposed
- Schematic floor plans
- Detailed architectural drawings showing building materials and their location on the building and relevant details
- Details of exterior lighting, windows, doors and any exterior features
- Samples or details of all materials proposed for the exterior of the building, including color chips, are required
- A model, 3-D rendering or perspective drawing

- showing materials and color designations
- Contextual analysis of how the building relates to the neighborhood which could include photos, site sections, or other visual details
- Any other materials to further explain the building and site in support of its design concept. This may include attendance of your design professional

The more detail provided in support of an application, the greater likelihood of a quicker and smoother decision-making process. Incomplete applications may be delayed until the omitted material(s) are received.

In addition to those supporting materials supplied at the time of application, new projects with a building footprint of 10,000 square feet (7,500 square feet in the Old Town Districts) or greater must provide a mock-up for review and approval on-site prior to material installation. In addition, a mock-up may be required as a condition of approval of any project where materials submitted during the formal application period are deemed to require detailing review.



MOCK-UP INSTALLATION REQUIREMENTS

The purpose of a mock-up is to provide visual confirmation of the design intent not otherwise portrayed at the time of formal application.

Requirements.

A mock-up must accurately represent the applicant's final material selections, the material installations and must include the following information:

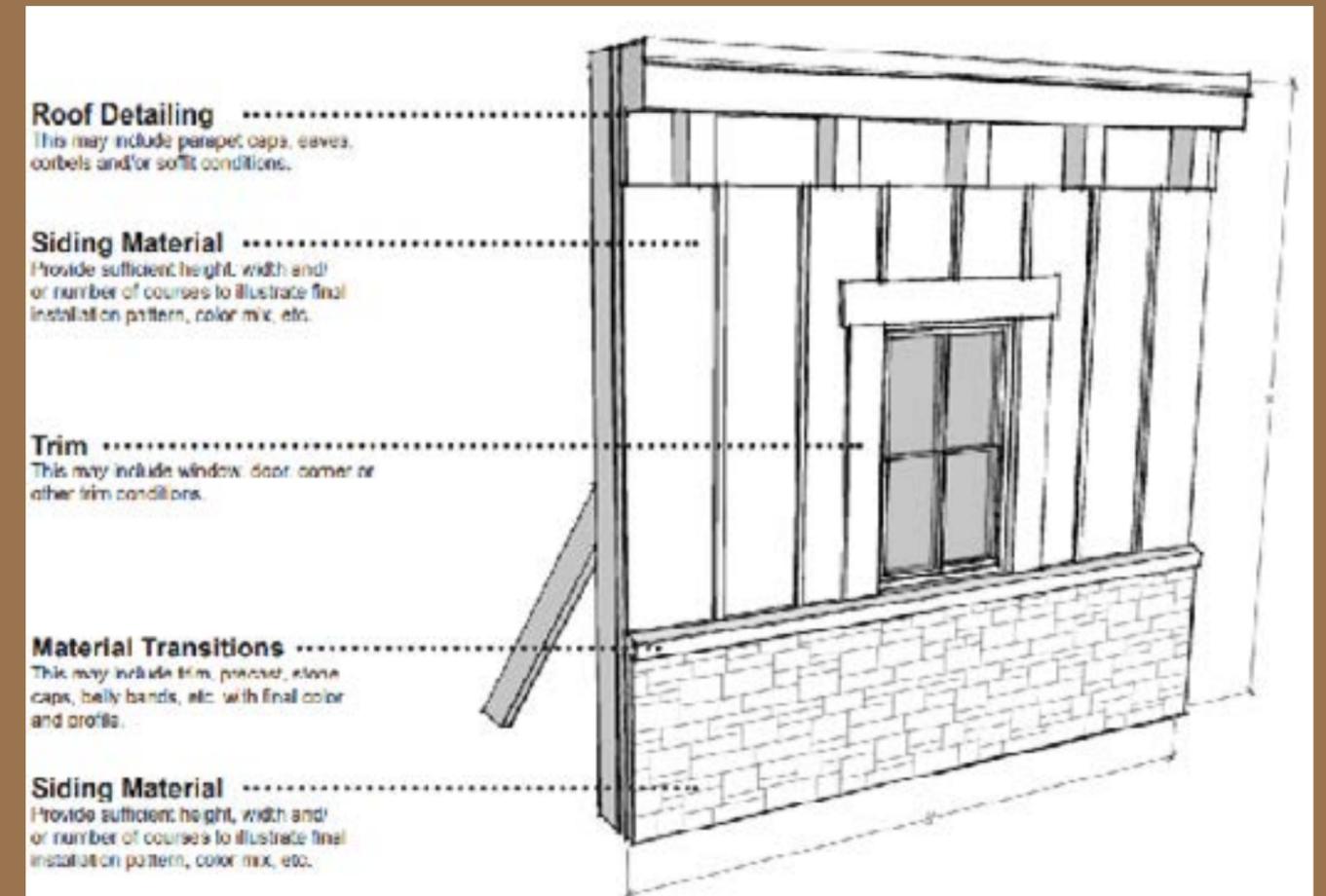
- Size – The dimensions are not stipulated but should be adequate to show the details of the materials – 8-feet x 8-feet is typical.
- Materials – Show all typical construction on the building façade, including but not limited to primary siding materials, siding transitions, trim, exposed flashings, typical fenestration, eave, canopy or other major appurtenance detailing. Full-scale materials should be provided where feasible.
- Detailing – Accurate representations of transitions between materials must be provided.
- Color – Accurate color with variation patterns such as variegated brick or stone must be shown.

Scheduling for Review.

The applicant is responsible for scheduling Committee review of the mock-up during the time of construction on-site but must be prior to exterior materials installation. The review will occur with a quorum of the Committee and City staff.

Committee Review.

In accordance with Section 2.7.3. Committee Review, the Committee must make a recommendation to staff to approve, table or deny the mock-up. If the mock-up matches the formal application, review must be reasonably limited to color, detailing and information not otherwise supplied during initial approval. Substitutions of materials made after formal application and project approval must be reviewed during a Committee meeting as an amendment to the original application and not during the mock-up review. Partial mock-up approvals may be considered by the Committee in order to allow construction to continue. Those items tabled or denied must not be installed until final approval is provided.



2.8 Committee Review and Recommendation

The Committee will review each project based on the adopted standards using the Project Review Checklist:

- ✓ Design Fundamentals
- ✓ Broad Design Principles
- ✓ Site and Landscape Plans
- ✓ Building Material Selection and Application
- ✓ Design Details
- ✓ Colors
- ✓ Lighting
- ✓ Accessories

The purpose of the Project Review Checklist is to look at projects from the broader design standards down to the smaller details. If the Committee finds items in one category are not met, discussion in the next category may not occur until the larger issues can be resolved to the satisfaction of the Committee.

Design fundamentals include issues surrounding the basic requirements within the Standards, such as which area of town the project is located. Is the project proposing a garage forward design? Does the project contain more than one 2-unit townhouse, triplex, fourplex or multi-family structure? What are the proposed materials and colors? How does the design solution respond to the neighborhood context, environment, history and future?

Principles of good design are the cornerstone to a vibrant, functional, and pleasing community. Projects should exhibit an understanding of procession, entry, form, space, mass, scale, hierarchy, proportion, circulation, rhythm, order, and simplicity.

Good design is cohesive and thought through prior to submittal without key components being deferred to being resolved in the field.

The Committee will come to one of the following recommendations for staff at the time of the meeting:

- Approval (with or without minor conditions),
- Table, pending submission of revisions or additional materials detailed at the meeting, or
- Denial

Staff will provide a written decision upon receipt of the Committee's recommendation within ten business days of the recommendation along with findings of fact supporting the decision.

2.9 Expiration

Once a project receives Architectural Review approval, it will be valid for 18 months. A building permit must be submitted to the City before the expiration date. The zoning administrator may grant one 6-month extension provided the applicant can demonstrate progress is being made on the project.

2.10 Re-Submittal of Application or Changes to an Approved Submittal

Minor amendments to an approved plan will be scheduled before the Architectural Review Committee for recommendation. No additional fee will be required for this review. The applicant will be notified of the Committee's recommendation at the meeting followed by a decision in writing from staff after the meeting. A minor amendment is a cosmetic or aesthetic change including, but not limited to, color and material changes.

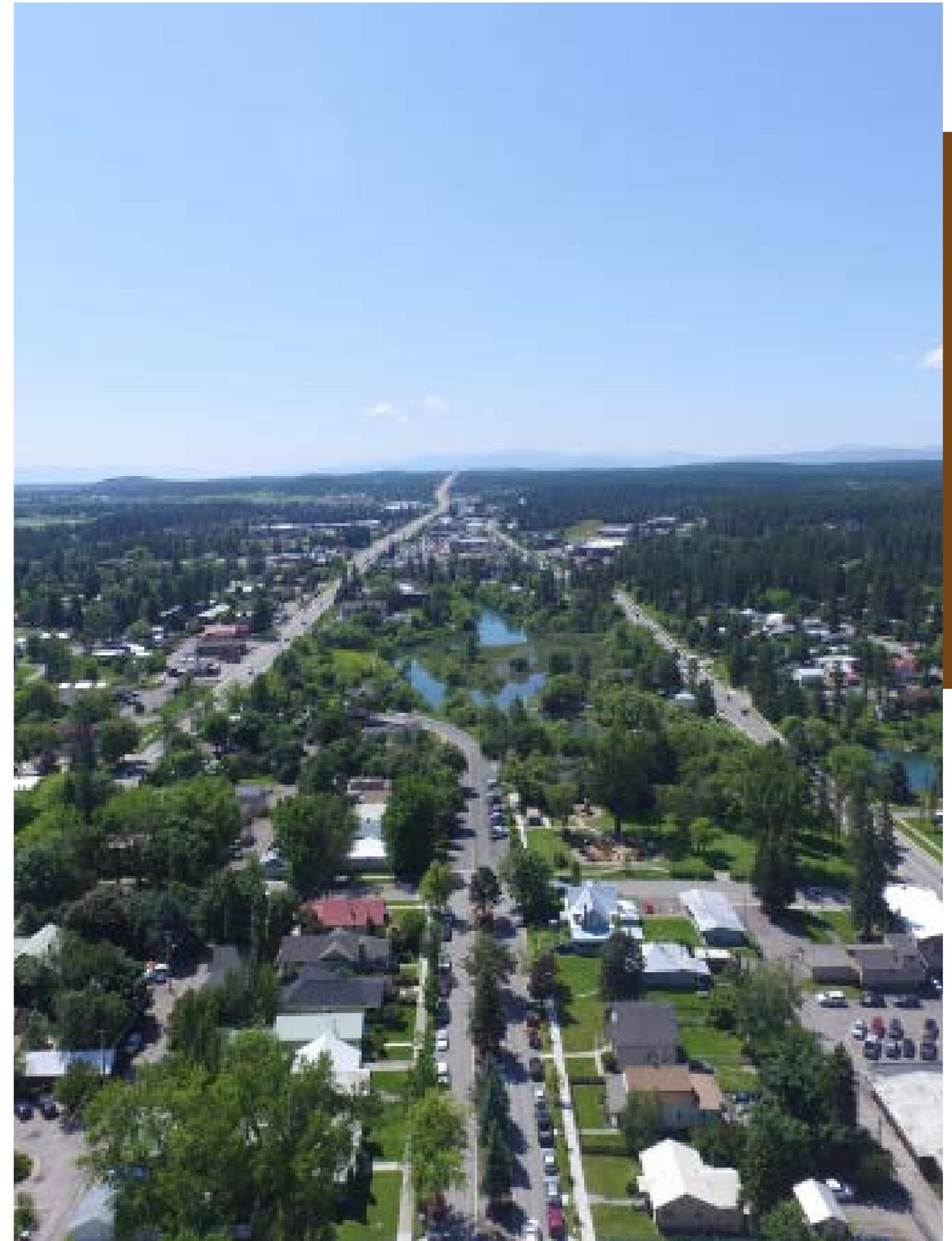
Major amendments to an approved plan, as determined by the Planning Director or designee, must be re-submitted as a Formal Application and a new review fee paid. A major amendment is a significant change including, but not limited to, a change to the site plan, the building footprint, the mass or scale of the structure.

2.11 Right of Appeal

Appeals of the staff's written decision must be made pursuant to Sec. 11-7-6, WCC.

2.12 Variances

Any variances to these standards must be made to staff, upon recommendation from the Committee.



HIGHWAY DISTRICT

Chapter 3

The Highway District is located along Highway 93 South from the Whitefish River south of the intersection with Montana Highway 40, as properties are annexed into the city limits, and are zoned WB-2: Secondary Business District, WI: Industrial and Warehousing District, WB-4: Business Park District, WB-T (Business Transitional District) and WBSD (Business Service District). This area is the gateway into Whitefish. The character of this area is retail, office, high density residential and light industrial uses on large lots. This area has evolved into an area predominately serving the automobile while providing limited opportunities for users of other modes of transportation such as bicyclists and pedestrians; however, this trend is changing with the installation of sidewalks and bike paths along the Whitefish River. This area typically has a need for large display, storage and/or parking areas. Areas zoned for Industrial uses will be reviewed under this design district section. In addition to these standards, the Mixed-Use and Non-Residential Building Development Standards (§11-3-43, WCC) apply.

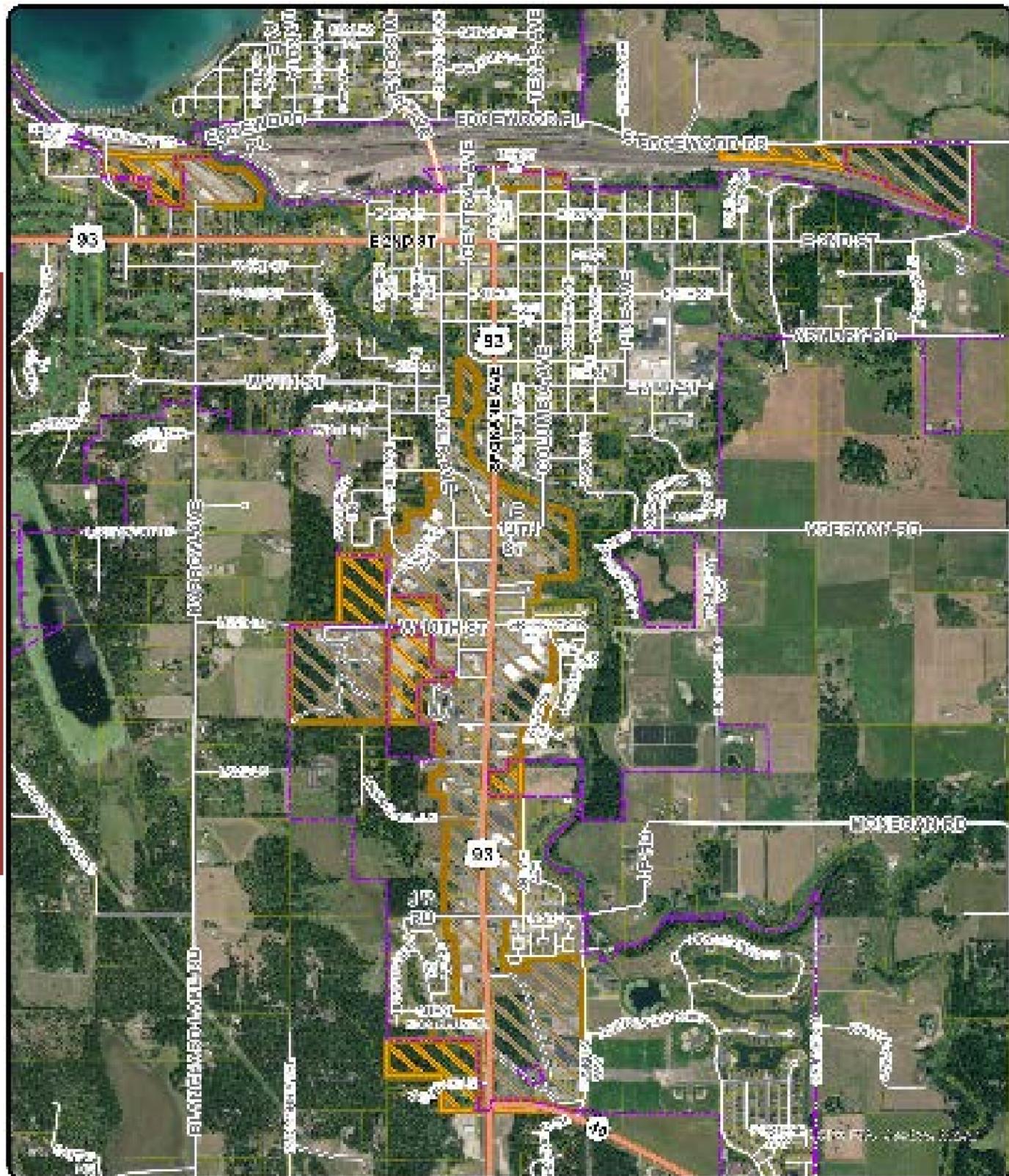
GOALS:

- The Highway District is the first impression many people have of our community. Site design should include inviting features such

as boulevards and well-dispersed landscaping throughout parking areas.

- Create a soft welcoming entry into our City and minimize parking lot impacts. Try to design commercial development into village-type units limiting the vast expanse of unbroken facades. The use of green belts and vegetation should be used to soften these environments. This may include design of small pocket parks to encourage employees and the community to embrace the outdoor environment.
- Orient buildings toward public streets.
- Parking lots will be located to the side and rear of lots with smaller “village-type” complexes abutting the highway.
- The site design should work to buffer parking lots from neighboring properties; earth berms and hedges are useful tools in achieving this goal.
- Infill and adaptive reuse projects are encouraged and shall complement the characteristics of the existing buildings built in the District since the adoption of the Standards in 2003.

The Highway District will continue to develop as an area with larger retail and services geared toward the day-to-day needs of residents and visitors of Whitefish. In addition to automobiles, sites within this area will take into consideration pedestrians and bicyclists. Sites will have significant landscaping to bring balance to larger buildings and parking areas of this district. The buildings may be larger than those found in other areas of the community but will continue to reflect a small-town scale.



0 25 50 100 Feet

City of Whitefish



Date: 09-20-11

Architectural Design Districts

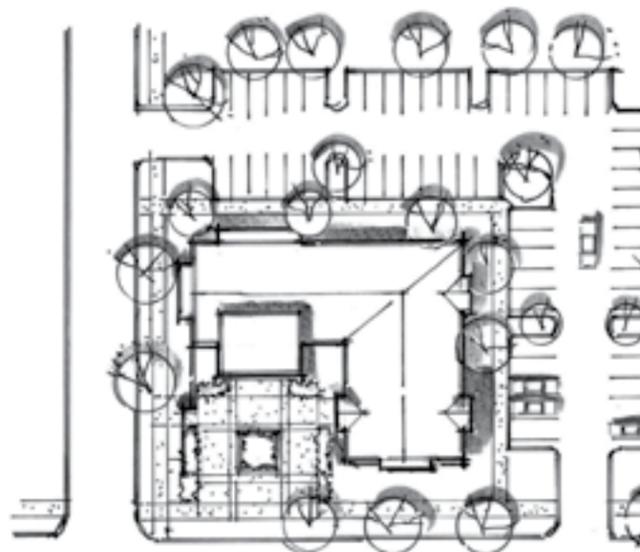
- Highway District
- Highway District County



Outdoor Space for Patrons

3.1 Site Design Standards

3.1.1 Buildings must be arranged on the site so their orientation frames, encloses or otherwise gives prominence to a pedestrian corridor, an outdoor gathering space with outdoor seating, a “main street” pedestrian or vehicle access corridor within the site or the corners of the street intersections or entry points into the development.



Parking to Side and Rear of Project



Covered Bike Parking

3.1.2 Buildings on corner lots should be oriented toward the primary intersection and the primary and secondary street frontage, while parking and auto access must be located away from the primary intersection corners. The use of corner entrances, plazas, signage and/or landscaping is encouraged to accentuate corner sites.

3.1.3 Where buildings are separated from the public sidewalk along the primary street frontage, the space should contain public and pedestrian amenities. Buildings may be separated from the sidewalk by plazas, landscaping, benches, bicycle racks, trash receptacles and other pedestrian amenities.

3.1.4 The layout of the site must consider pedestrians and bicyclists, as well as automobile traffic. Pedestrian access from adjacent sidewalks must have priority over vehicular traffic. Consider protection from the weather.

Consider how a pedestrian or a bicyclist would access the proposed site. Mixing nonmotorized transportation in the same spaces as automobile traffic can be a very unpleasant experience for both pedestrians and bicyclists. Consider alternate and/or separate routes into the development that clearly define the pedestrian area versus areas for vehicles.

3.1.5 Minimize the dominance of parking surfaces and structures. Use green spaces to soften the “sea of asphalt” which often dominates commercial “strip” development. A close review of the City’s landscaping ordinance is highly recommended. Consider building orientation and its relationship to the entire site.

3.1.6 All new development must invite pedestrian access. Thoughtful site layout and the use of “green open space” should be included in all site planning. The front door/main entrance to a development must be easy to identify to the pedestrian and the buildings must be oriented toward the primary street

front and public path and/or sidewalks. This may include providing a direct access from a public sidewalk to the front entrance.

3.1.7 Make site more appealing to pedestrian and bicycle traffic and use. Thoughtful application of design principles and space planning can make a site more pleasant to view and a better safer place to be. The use of courtyards, decks, traditional front porches and bicycle amenities are design tools to accomplish this goal. Provide bicycle parking areas. Consider protection from the weather.

3.1.8 Fencing should be of a natural material. The use of chain link and vinyl should be avoided but may be considered by the Committee.

3.1.9 Site design must consider the seasonal nature of the Whitefish climate allowing for summer enjoyment and snow storage in the winter. Also, consider southern exposure, energy costs and impact of shadows on adjacent properties.

3.1.10 Promote safe ingress and egress. Consolidate site access to public rights-of-way to reduce congestion and the need for endless stoplights. The City seeks to promote shared access where possible. Provide cross-property easements to share driveways and reduce the need for additional curb cuts, when feasible. (§11-3-43E(5), WCC)

3.2 Landscaping Design Standards

Goal: Considerable investment has occurred along Highway 93 South to create an entrance into our community. It is expected those developing property in these areas will recognize this investment and meet or exceed this landscaping standard. Thoughtfully designed landscaping will contribute to a pleasing character for the development and Whitefish. Landscaping should not be used just as a visual screen but should also encourage a sense of place and connection to the environment.

Standards:

3.2.1 Preserve and maintain mature trees and existing vegetation as much as practical.

3.2.2 A thoughtful selection of appropriate vegetation and landscaping materials. The designer should carefully select hardy vegetation and materials to survive our harsh

climate; while also accentuating the natural beauty of our community. Use native, drought-resistant species and plants requiring low amounts of water, chemicals and fertilizers.

3.2.3 Landscaped areas must be planned as an integral part of the project, rather than simply located in the left-over spaces on the site.

3.2.4 Landscaping should complement the architecture of the project and adjacent buildings.

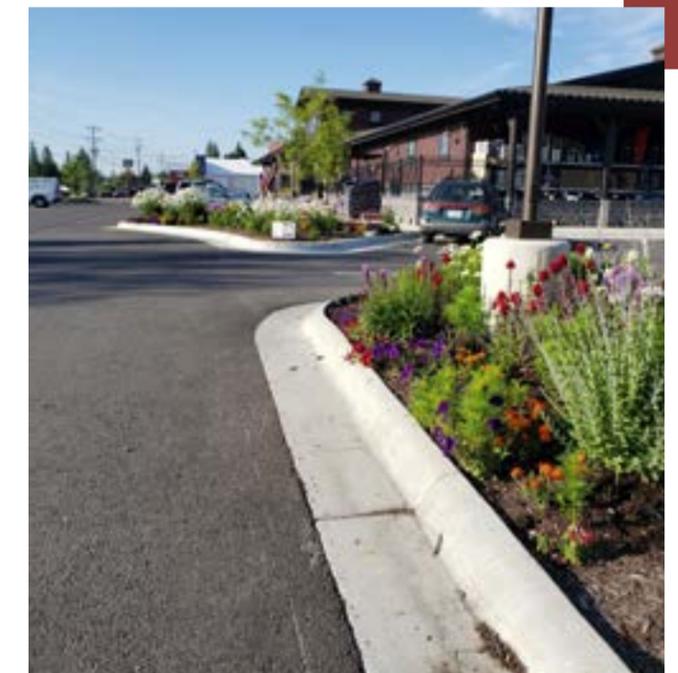
3.2.5 Landscaping (along with the location of the building) should soften the appearance of the site as seen from the roadway.

3.2.6 It is beneficial and attractive to incorporate outside seating areas in the landscaping plan.

3.2.7 In a landscape plan, attention should be paid to the plant material’s color and texture to create unity on the site. Simple masses of material with one predominant species provide unity, while accent plantings provide attractive contrasts.

3.2.8 Species should be planted which are suitable to the space they will occupy when they have achieved their full growth. This includes understanding the growth of species root mass and their effect on sidewalks, utilities and building foundations.

3.2.9 Plants with multi-season interest provide color and interest throughout the year.



Thoughtful Parking Lot Landscaping

3.2.10 Plazas and malls should be planted to reflect an informal place suited to pedestrian scale. Variety and color are encouraged.

3.2.11 Landscaping and earthen berms or sight obscuring fencings must be used to screen equipment, refuse areas and storage areas. It is better to mass plant material in groups rather than locate them in a straight line which tends to look unnatural, except when required to provide a landscape buffer as defined in the Landscape Requirements. (§11-4, WCC)

3.2.12 The landscaping depicted on renderings/ perspectives should be a realistic interpretation of what will be installed at the time of planting.

3.2.13 Provide uniform planting of required street trees.

3.2.14 The final landscape, irrigation and tree preservation techniques shall be reviewed and approved by staff prior to issuance of a building permit.

Note: See Whitefish Zoning Regulations for the Landscaping requirements. (§11-4, WCC)

3.3 Off-Street Parking Design Standards

Goal: Parking lots in Whitefish will be designed as a necessary appurtenance of a site and not the primary visual emphasis. Parking must be located to the side and/or rear of a building.

Standards:

3.3.1 The use of large parking lots as the primary feature at the entrances of businesses is not allowed.

3.3.2 Parking must be distributed throughout the site and located to the side and rear of the building. No parking may be located between the buildings and the primary street frontage. Parking lot design should consider a southern aspect to reduce the accumulation of ice during the winter and allow for natural melting.

3.3.3 Parking lots shall be designed to consider all users and not only automobiles. The development must minimize the impact of parking on the building's relationship to the street and pedestrian-oriented character and character of the neighborhood. Secure, well-lit and convenient bicycle parking and storage must be provided.

3.3.4 The use of side streets for the primary entrance is required when feasible to promote safe ingress and egress, and to prevent the location of parking lots along the primary traffic artery. Provide cross-property easements to share driveways and reduce the need for additional curb cuts, when feasible. At a minimum, a pedestrian connection between



Parking to Side of Project



Parking to Rear of Project

parking lots between developments must be explored. An automobile connection between parking lots must also be provided where physically possible.

3.3.5 Parking areas shall be designed to minimize the lot's visual impact, thus creating an architectural form instead of a wasteland.

3.3.6 Parking structures built below grade are desirable because they less obvious to passers-by, and are easier to screen with landscaping.

3.3.7 Soften large parking areas with landscaping, earthen berms and pedestrian friendly features.

3.3.8 The amount of unrelieved pavement on parking lots shall be limited by methods such as use of landscaping, contrasting colors, and pathways of alternate paving material.

3.3.9 Landscaping treatments of parking lots should include trees both on the perimeter and interior of the lots.

3.3.10 Trees in parking lots should be protected from vehicle damage by concrete curbing or other effective means and must provide an adequate root zone uncompacted by vehicular traffic. In high traffic areas, the use of subsurface tree root protection is recommended.

Note: See Whitefish Zoning Regulations for the Off-Street Parking requirements. (§11-6, WCC)

3.4 Exterior Lighting Design Standards

Although there are many needs for lighting in our built environments, obtrusive aspects of lighting often extend well beyond the boundaries of the area in which the lighting is installed and intended for use. These obtrusive aspects, such as glare, light trespass, energy waste and sky glow, can have serious consequences for the public health, safety, and welfare. They can be effectively controlled or eliminated with carefully considered attention to design, installation, and use.

Goal: To provide thoughtfully designed outdoor lighting adequate to promote safety and security.

Standards:

3.4.1 All outdoor lighting must be carefully designed to light only the area needed for reasonable levels of safety and security. Seek to eliminate as much outdoor lighting as possible.

3.4.2 Light placement must avoid placing glare on neighboring properties. Every attempt should be made to consider the impacts the additional lighting will have on the surrounding environment

3.4.3 Where appropriate, the use of ground lighting should be considered. Consolidate exterior lighting and minimize the height and projection of the lamps where possible.

3.4.4 Building Lighting

a) Exterior lighting shall be architecturally integrated with the building style, material and colors.

b) Exterior lighting may be mounted flush or projected from the building wall.

c) Neon, bright colors or flashing lights are not acceptable building lighting and are not permitted.



Acceptable Painted Pole Base Complementing the Business



New Commercial is a Similar Scale to Residential Structure Behind

3.4.5 Site Lighting

a) The design of the site lighting should complement the architectural features of the building. Consider the use of the standard City streetlights; however, the use of other lights may be considered by the Committee.

b) Fixture mounting height should be appropriate for the project and the setting. Use of low, bollard-type fixtures, 3 to 4-feet in height, are encouraged as pedestrian area lighting.

c) Raised light pole bases shall be attractively designed and well detailed to be compatible with the overall project. The use of “sonotube” (exposed concrete) bases is discouraged. These foundations, if used, need to be treated in some fashion to cover the exposed concrete.

d) The placement of light poles within raised curb planter areas is encouraged; however, conflicts with parking lot trees which can obscure the lighting should be avoided

e) Swivel-mounted luminaries are prohibited.

Note: See Whitefish Zoning Regulations for the Outdoor Lighting Standards. (§11-3-25, WCC)

3.5 Building Design Standards

All building designs should be a positive complementary enhancement to the existing architecture, quality of life and character of the Whitefish community and particularly the Highway District. The positive enhancement should be reflected in the design elements of the project. These design elements are both aesthetic and practical and include: 1) scale, character and relationship to surrounding neighborhood 2) shape and form; and 3) exterior materials. Due to their massiveness, buildings greater than 10,000 square feet have

additional design standards found in Section 3.9. Also, businesses considered formula retail (also known as franchise businesses) may not use their traditional building design or color scheme. See Section 3.10 for these Standards.

3.6 Scale and Relationship to Surroundings

3.6.1 All buildings must have a scale in size, height and mass that relates to the surrounding buildings and adjacent commercial development.

3.6.2 The size, height and mass must not detract from, conflict with or overwhelm the surrounding neighbors.

3.6.3 Distance from other structures and features will be considered in reviewing scale. For example, The WAVE works well at its location; however, if it were on a lot next to Valley Glass, it would be too overwhelming.

3.6.4 Building site placement and size should minimize obstruction of views to the mountains and river.

3.6.5 Mixed-Use Non-Residential Development Adjacent to Residential Zones. Buildings must be designed to ensure building massing, height and scale provide sensitive transition to adjoining residential neighborhoods. When abutting a residential zoning district, the project’s landscaping plan must include provisions for vegetative screening between the project and the residential property.

New developments adjacent to residential areas must mitigate impact through careful site planning and architectural design. Possible mitigation techniques include, but are not limited to:

- Locating open space and preserving existing

Complementary Scale



vegetation on the site’s edges to further separate the building from less intensive uses

- Stepping down the massing of the building along the site’s edges
- Limiting the length of or articulating building façade to reflect adjacent residential patterns
- Creative use and ongoing maintenance of landscaping such as berms, buffers, mounds, rockeries, living fences and swales designed to avoid the appearance of a straight line of “wall” or uniform plant material and must be wide enough to accommodate the mature plants

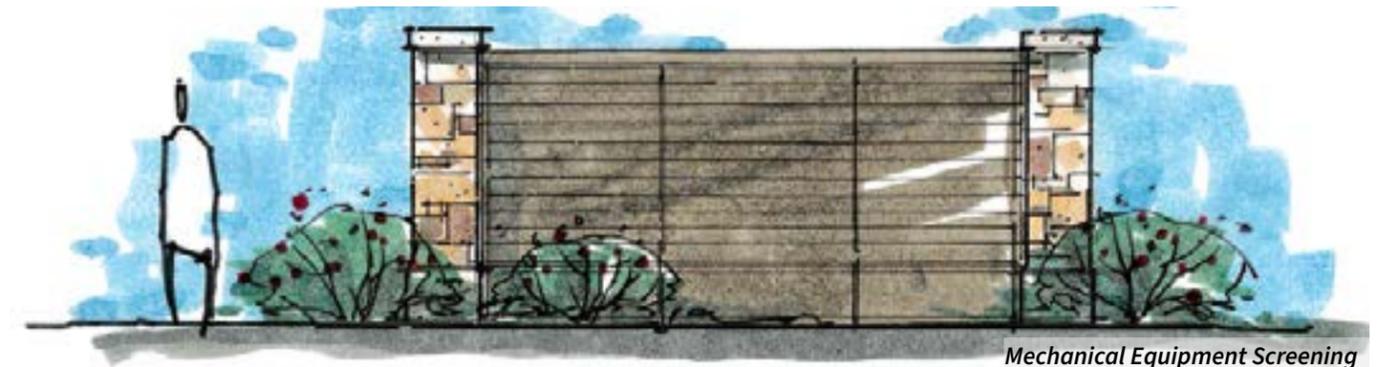
3.6.6 Multiple Building Projects

a) Buildings must exhibit a general similarity of scale, orientation, and proportion with unified natural and built features. As a rule, taller buildings must be placed toward the interior of the site and stepped back from the street. Buildings must be grouped or sited with other buildings to create distinct outdoor spaces, with distinct pedestrian connections between the buildings, parking and the street. Sites must connect to natural corridors and trail systems and must facilitate pedestrian and bicycle traffic.

b) Projects with multiple buildings to be built in phases on one site must include design information for each building. Initial buildings must meet all the requirements of the Architectural Review Standards. Information on buildings to be built in the future must include an entire site development narrative with a site plan indicating buildings, locations, access for parking and pedestrians, general landscaping and concept of building sizes, shapes, materials and colors.

3.7 Shape and Form

All buildings shall have a shape and form regarding rooflines and massing that relates to the nearby buildings compliant with the Architectural Design Standards. Rooflines should be pitched, pitched with false fronts, flat, flat with false fronts, and shed with false fronts, hipped, gable or gambrel. Overhanging eaves are encouraged. Flat roofs shall have parapets.



Mechanical Equipment Screening

3.7.1 All remote mechanical and electrical equipment (condensers, chillers, intake/exhaust fans, cabinets, etc.) regardless of horsepower, must be enclosed within sound absorptive walls, as permitted by the Building Code. These areas shall be located and screened so the visual and acoustic impacts of these functions are fully contained and out of view. Enclosures shall be reviewed by the Committee where applicable and the acoustic design will be reviewed and approved at the time of plan examination by the Building Department.

3.7.2 Buildings in this area are unique as they are set back a minimum of 20-feet from the property line. These buildings are on larger lots than downtown, and usually require large display or parking areas. Because of these conditions, these buildings are usually larger than those downtown. In order to maintain an aesthetic quality compatible with Whitefish, structures must be designed to minimize the massing of the building. This can be accomplished by breaking up large expansive walls and rooflines by adding dormers, recessed wall lines and varied roof heights.

3.7.3 Upper story windows should maintain the same vertical characteristic as downtown.

3.7.4 Store fronts must be aesthetically pleasing with covered entryways, walkways, outdoor courtyards and sitting areas.

3.7.5 Buildings must have roof designs that do not cause snow or drainage problems for themselves or adjacent buildings and sites.

3.7.6 When the backsides of buildings face a street (as on Baker Avenue) they must be aesthetically developed. Large blank walls with no detail are not acceptable.

3.7.7 Upper story balconies are encouraged.

3.8 Exterior Materials

3.8.1 Exterior finish building materials shall be of a nature in color and texture complementary with the structures within each building district. They shall not clash, detract or conflict with adjacent buildings compliant with the Architectural Design Standards.

3.8.2 Acceptable exterior cladding materials should include properly scaled brick, 1x4, 1x6 or 1x8 horizontal natural wood siding, sawn shingles, 1x6 or 1x8 vertical channel natural siding, 1x8 to 1x12 vertical board and batten natural wood siding with 1x2 batts, natural stone veneers, and stucco with wood trim. Other fire resistant and ecological friendly materials may be considered.

3.8.3 Wood shingles, vertical wood siding and boards with batts are acceptable in gables and upper portions of walls.

3.8.4 Exposed plain concrete block or vinyl cladding are not allowed. Metal siding, Exterior Insulation Finish System (EIFS), composite siding and stone panels are discouraged but may be used at the discretion of the Committee.

3.8.5 Acceptable roofing materials shall be asphalt composition shingles, metal, or appropriate flat roofing. Other technologically advanced products such as solar roof tiles are encouraged.

3.8.6 Roofing shall be of colors that complement neighboring structures.

3.8.7 Mirrored and tinted glazing is not allowed. Low-E glass is acceptable. Vision glass on

building sides facing streets is encouraged.

3.8.8 Exterior classic details and elements such as brackets, cornices, window trim, and wall corner boards are encouraged.

3.8.9 Higher quality and easily maintained materials are strongly recommended.

3.9 Commercial Design Standards for Buildings 10,000 Square Feet or Greater

As Whitefish is generally characterized by buildings small in scale; buildings with a footprint of 10,000 square feet or greater require an extra level of review to ensure they fit into the community. Additional site and design components will be required as part of the overall design of the site.

3.9.1 Site Design Standards

a) Landscaping.

The parking lot landscaping requirements shall be twice the amount required under §11-6-5, WCC.

b) Pedestrian and Bicycle Amenities.

Pedestrian Lanes Required. Walkways shall be integrated into the design of the site. The location of the walkways must maximize the ability of a pedestrian to safely maneuver through the parking lot by serving the greatest number of parking spaces and walking to adjacent land uses. Their location must consider adjoining properties, and the location

of transit/Shuttle Network of Whitefish (SNOW) Bus stops. At a minimum, the pedestrian lane must connect the building with the parking lot and the street.

Connections shall be separated from the parking lots and drive aisles by grade, landscaping or other techniques and shall be a minimum of 6-feet wide. Such connections shall continue across driving lanes by use of a material other than asphalt. Acceptable material choices include concrete or colored concrete with textures imbedded into the concrete. Painted stripes are not acceptable.

Every parking lot must have at least one pedestrian lane. Additional pedestrian lanes must be provided at a ratio of one lane per four rows of parking.

Bicycle Parking Required. A bicycle rack shall be provided with a minimum of four parking stalls. Bicycle facilities should be shared among adjoining establishments. Bicycle racks which only support a bicycle front or rear wheel are not permitted. The rack must be securely mounted to the ground and covered. Bicycle parking spaces should be 2-feet by 6-feet with no less than a 7-foot overhead and a 5-foot maneuvering aisle behind each row of bicycle parking. A bicycle parking area should be separated from a motor vehicle parking area by a barrier, post or bollard, or by at least 5-feet of open space behind the maneuvering area. Bicycle facilities should be well-lit and located no further from a public entrance than the nearest non-handicapped parking stall. If public bicycle parking is not clearly visible from the main entrance, then directional signs should be provided.

Commercial Village



4-Sided Architecture



Big Box Building with Sidewalk Connection

Big Box Building with Transit Stop



c) Transit/Snow Bus Stops.

If a transit/SNOW Bus stop is located on the site or if there are plans to permanently establish a stop on the site (contact the SNOW Bus and Eagle Transit), the following standards shall be designed into the site:

- The plaza for the stop shall be sized to accommodate pedestrians using the sidewalk as well as those waiting for the bus and the required amenities. The plaza shall be no smaller than 200 square feet and shall be designed in contrasting materials.
- Seating for a minimum of three people shall be incorporated into the plaza. More seating may be needed based on the usage of the stop.
- Each plaza shall include a refuse container.
- Each plaza shall incorporate a shelter for skiers and a place for snowsport equipment to be secured. The shelter shall be installed and paid for by the developer. The shelter design will be determined by the City.
- Dark sky compliant lighting shall be incorporated into the transit/Snow Bus stop.
- Transit/SNOW Bus stops shall have a pedestrian connection to the main building on-site either through a sidewalk adjacent to a right of-way or through a separated pedestrian lane.
- Maintenance of the plaza shall be the

responsibility of the property owner(s).

d) Service, Loading, and Refuse Areas.

Service, loading and refuse areas are a required component of any building but can severely detract from the design of the building and the pedestrian experience. Service and loading areas shall not be located between the building and the street unless there is no possible alternate location. Refuse shall not be located between the front of the building and the street. In no instance shall refuse and service/loading areas be located adjacent to residentially zoned areas.

Screening of refuse containers and service areas from public view shall be required in the form of dense landscaping and/or walls constructed of a complementary color and material to the main building and shall be no less than 6-feet tall.

3.9.2 Building Design

Avoid flat walls through building modulation to reduce the mass and bulk of the structure. This can be achieved by utilizing the following:

- Building setbacks on upper floor levels;
- Corners shall have special massing and architectural treatment;
- Building ornamentation;
- Varying roof lines, pitches, and shapes;
- Vertical modulation to break down the scale of large one-story;
- Overhangs, awnings and marquees;
- Dormers, balconies, porches, staircases; and/or
- Window and door fenestration.

a) Building Equipment.

Building equipment located on the top or sides of buildings shall be completely screened from view.

b) Blank Wall Limitation.

In order to reduce blank wall impacts on the pedestrian and business environment, buildings shall employ a cohesive design strategy to mitigate their appearance every 40-feet. These items may include but are not limited to recessed walls, projections, reveals, projecting ribs, windows, display cases, landscaping,

trellises, or other architectural features.

Secondary walls more than 15-feet in length without windows, entry, architectural features or modulations shall not face any street (public or private), alley or parking lot.

c) Materials.

Use aesthetically pleasing and compatible materials and colors. This could include brick, wood, sandstone, other native stone and tinted, textured concrete masonry units. Prohibited materials include smooth faced concrete block, concrete tilt-up and prefabricated steel panels. For larger projects, approved materials must be presented in a mock-up. See Section 2.7.3.

d) Entryways.

Careful consideration shall be given to the design of the entryway. Incorporated features may include canopies or porticoes, overhangs, recesses or projections; arcades, peaked roof forms; display windows and integral planters.

e) Buildings on Corners.

Buildings on corners shall be located as close to the right-of-way as possible in order to frame the street. Special architectural elements shall be incorporated to accentuate the building's prominent location.



3.10 Formula Business Design Standards

Commercial uses meeting the zoning definition of "formula business" are expected to minimize generic and corporate images, and express the architectural elements and forms that best express the values and heritage of the community.

3.10.1 Formula business must not use standard corporate building shapes, forms and rooflines.

3.10.2 Formula business color schemes are not acceptable.



Examples Non-Standard Franchise Architecture

OLD TOWN DISTRICT

Chapter 4

The Old Town District is the main downtown area of Whitefish and is zoned WB-3: General Business District. This area is the center of financial, retail, commercial, governmental, professional, institutional and cultural activities for the community. The character of the area is retail, professional office and residential uses. This area is also the focus of the Downtown Business Master Plan, which reinforces the downtown as the center of the community and identifies certain public and private investments in order to maintain the strength of the downtown. The intent of the Old Town District is to blend the existing historical feel of the area with new small professional offices and retail. In order to maintain the sense of the area as a small community within a community, the emphasis on vehicular traffic should be downplayed in design.

There are three distinct Design Districts within the Old Town District - the **Central**, **South** and **Railway** Districts.

The **Central District** is located on Central Avenue between East 4th and Depot Streets, west side of Spokane Avenue between East 4th and East 3rd Streets, both sides of Spokane Avenue from East 3rd Street to Depot Street, east side of Baker Avenue between East 4th and East 3rd Streets, both sides of Baker Avenue between East 3rd and Railway Streets. This area is the heart of the Old Town District and is characterized by 1½ to 2 story mixed-use buildings with retail on the

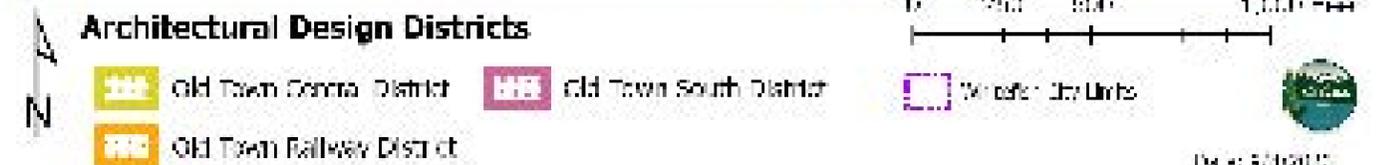
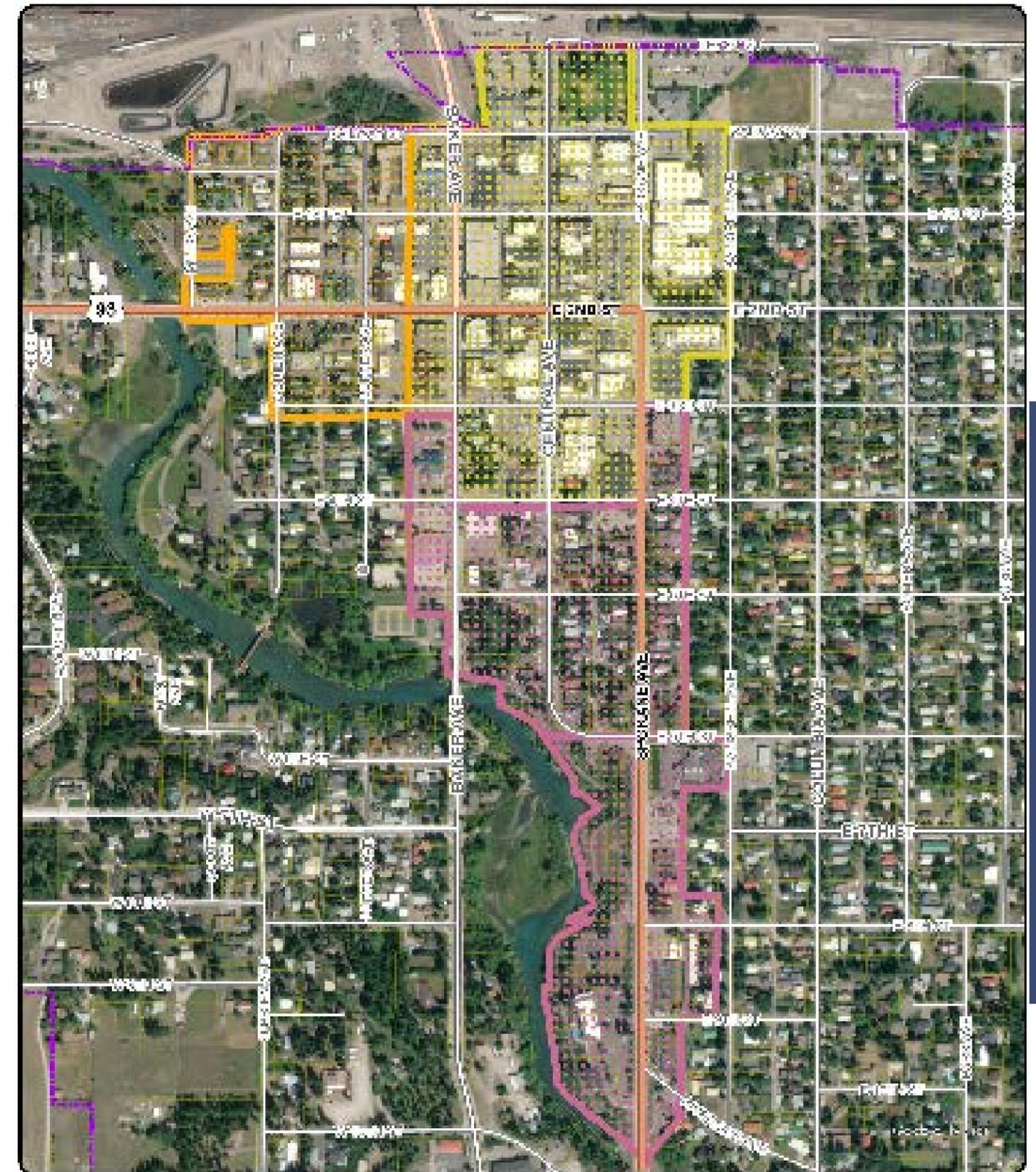
main floor and offices or residential on the 2nd story.

The **South District** is located along the east side of Spokane Avenue between East 3rd and East 4th Streets, both sides of Spokane Avenue from East 4th Street to the Whitefish River, Central Avenue between East 4th and East 7th Streets; the west side of Baker Avenue between East 3rd and East 4th Streets, and both sides of Baker Avenue from East 4th Street to the Whitefish River. Although some of this area is zoned for commercial uses, these areas were originally residential and functions as a transition from the Downtown to the Highway 93 South Corridor.

The **Railway District** is bounded by Railway Street to East 3rd Street and the east side of Miles Avenue to Lupfer Avenue. This area is characterized as historic residential with small scale buildings. The front yard setbacks are defined by the predominate front setback along the block in order to maintain the residential character of the district.

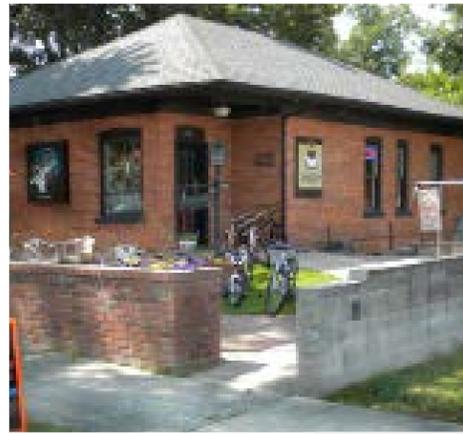
In addition, the Mixed-Use and Non-Residential Building Development Standards (§11-3-43, WCC) apply.

The Old Town District will continue to develop in a manner respecting the mass and scale of the existing buildings, the character of the downtown and the community vision. New buildings shall respect the uniqueness of each of the distinct design districts.





Corner Building



Outdoor Pedestrian Space



GOALS:

- Development must maintain a strong pedestrian scale and must focus on the people using the structure.
- Respect should be shown for the existing grid and neighborhood layout.
- New design should enhance the existing pedestrian walkways and serve to facilitate foot and bicycle traffic.
- Buildings should address the street and be designed to pull the interest of the users and act as a focal point of the greater neighborhood.
- Design attention should be paid to the pedestrian and bicycle building interface.
- The use of awnings and covered walkways are encouraged to promote year-round use.
- Alleys should be considered for primary entrances and utilized as alternate commercial spaces.
- Consider the pedestrian and cyclist as the primary user in all design. Avoid allowing the car and parking to drive design.
- Infill and adaptive reuse projects are encouraged and shall complement the characteristics of the existing buildings, including those built in the District since the adoption of the Standards in 2003.

4.1 Site Design Standards

4.1.1 All new development must invite pedestrian access. Thoughtful site layout and the use of “green open space” should be included in all site planning. The front door/

main entrance to a development must be easy to identify to the pedestrian and the buildings must be oriented toward the primary street front and public path and/or sidewalks. This may include providing a direct access from a public sidewalk to the front entrance.

4.1.2 Make site more appealing to pedestrian and bicycle traffic and use. Thoughtful application of design principles and space planning can make a site more pleasant to view and a better, safer place to be. The use of courtyards, decks, traditional front porches and bicycle amenities are design tools that can accomplish this goal. Consider protection from the weather.

4.1.3 Site design must consider the seasonal nature of the Whitefish climate allowing for summer enjoyment and snow storage in the winter. Also, consider southern exposure, energy costs and impact of shadows on adjacent properties.

4.1.4 Fencing should be of a natural material. The use of chain link and vinyl should be avoided but may be considered by the Committee.

4.2 Landscaping Design Standards

Goal: Thoughtfully designed landscaping will contribute to a pleasing character for the development and Whitefish. Landscaping should not only be used as a visual screen but should encourage a sense of place and connection to the environment. Landscaping is not required for sites in the Old Town District with buildings built lot line to lot line; however, well placed flower baskets, window boxes, hanging plants, green roofs or other opportunities for landscaping in the Old Town Central District are highly encouraged.

Standards:

4.2.1 Preserve and maintain mature trees and existing vegetation as much as practical.

4.2.2 A thoughtful selection of appropriate vegetation and landscaping materials. The designer should carefully select hardy vegetation and materials to thrive in our harsh climate, while also accentuating the natural beauty of our community. Use native, drought-resistant species and plants requiring low amounts of water, chemicals and fertilizers.

4.2.3 Landscaped areas must be planned as an integral part of the project rather than simply located in the left-over spaces on the site. For sites not required to provide landscaping, project proponents should look for opportunities to integrate landscaping in the site using planter boxes, hanging baskets or other creative means.

4.2.4 Landscaping should complement the architecture, building a cohesive relationship between the indoor and outdoor spaces.

4.2.5 It is beneficial and attractive to incorporate outside seating areas in the landscaping plan.

4.2.6 In a landscape plan, attention should be paid to the plant material’s color and texture to create unity on the site. Simple masses of material with one predominant species provide unity, while accent plantings provide attractive contrasts. Smaller spaces may be enhanced by the use of more compact varieties and a mixture of textures.

4.2.7 Species should be planted which are suitable to the space they will occupy when they have achieved their full growth. This includes understanding the growth of species root mass and their effect on sidewalks, utilities and building foundations.

4.2.8 Plants with multi-season interest provide color and interest throughout the year.



Downtown Landscaping



4.2.9 Plazas and malls should be planted to reflect an informal place suited to pedestrian scale. Variety and color are encouraged.

4.2.10 Landscaping and sight obscuring fencing must be used to screen equipment, refuse areas and storage areas. It is better to mass plant material in groups rather than locate them in a straight line which tends to look unnatural, except when required to provide a landscape buffer as defined in the Landscape Requirements. (§11-4, WCC)

4.2.11 Landscaping depicted on renderings/perspectives should be a realistic interpretation of what will be installed at the time of planting.

4.2.12 Provide uniform planting of required street trees.

4.2.13 The final landscape, irrigation and tree preservation techniques shall be reviewed and approved by staff prior to issuance of a building permit.

Note: See Whitefish Zoning Regulations for the Landscaping requirements. (§11-4, WCC)

4.3 Off-Street Parking Design Standards

Parking lots in the Old Town Districts are not required unless there is a residential component to the development. Off-street parking

developed on these lots will be at a much smaller scale than those found along Highway 93 South and may be designed off an alley, as opposed to the street.

Goal: Parking lots in the Old Town District will be designed to maintain the continuity of the active pedestrian streetscape.

Standards:

4.3.1 Locate a parking facility at the interior of a block and off an alley whenever possible. Parking shall be distributed throughout the site with a preference for parking to the rear of the building. Parking lots shall not be placed at corners, as these areas are generally more visible than interior lots, serve as landmarks and frame intersections. If this is the only place for parking, added pedestrian features shall be integrated into the corner such as a bench, plaza, art or an exceptional landscape feature.

4.3.2 The use of large parking lots as the primary feature at the entrances of business is not allowed. Where a parking lot abuts a public sidewalk a 5-foot wide landscaped buffer must be provided, according to the zoning, and consideration should be given to incorporating a low wall, no greater than 42-inches. Such a wall could be also be designed as seating.

4.3.3 New curb cuts and/or driveways eliminating on-street parking spaces are not allowed in the Old Town Central District and the Old Town Railway District unless there is no alley

access available. Restricting parking access on streets important for commercial retail reduces the impact of automobiles, improves auto circulation, preserves on-street parking and creates a more pleasant environment for pedestrians.

4.3.4 The use of side streets for the primary entrance is required when feasible to promote safe ingress and egress, and to prevent the location of parking lots along the primary traffic artery. Provide cross-property easements to share driveways and reduce the need for additional curb cuts, when feasible. Parking lot design should consider a southern aspect to reduce the accumulation of ice during the winter and allow for natural melting.

4.3.5 Parking structures built below grade are desirable because they less obvious to passers-by, and are easier to screen with landscaping.

4.3.6 Soften parking areas with landscaping, earthen berms and pedestrian friendly features.

4.3.7 The amount of unrelieved pavement on parking lots shall be limited by methods such as the use of landscaping, contrasting colors, and pathways of alternate paving material.

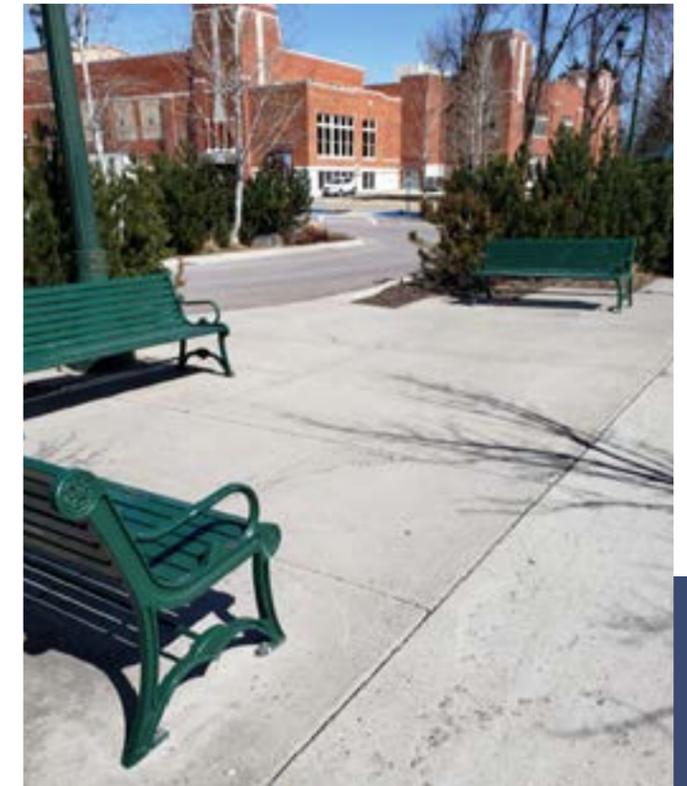
4.3.8 Landscaping treatments of parking lots should include trees both on the perimeter and interior of the lots.

4.3.9 Trees in parking lots shall be protected from vehicle damage by concrete curbing or other effective means and must provide an adequate root zone uncompacted by vehicular traffic. In high traffic areas, the use of subsurface tree root protection is recommended.

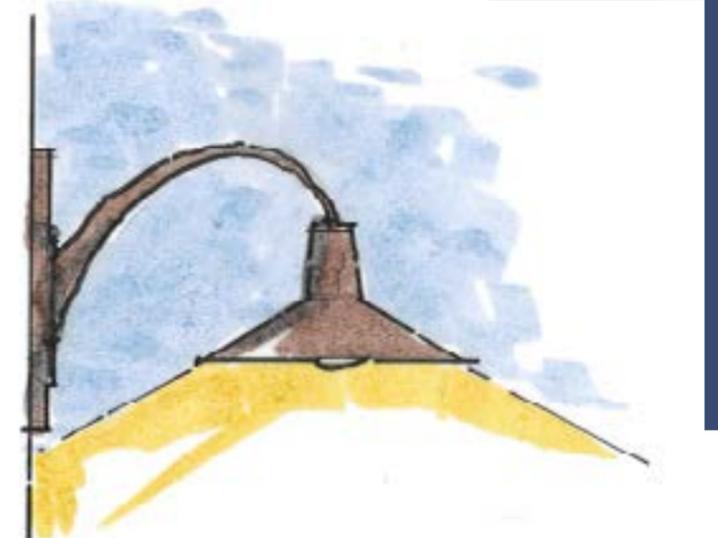
Note: See Whitefish Zoning Regulations for the Off-Street Parking requirements. (§11-6, WCC)

4.4 Exterior Lighting Design Standards

Although there are many needs for lighting in our built environments, obtrusive aspects of lighting often extend well beyond the boundaries of the area in which the lighting is installed and intended for use. These obtrusive aspects, such as glare, light trespass, energy waste and sky glow, can have serious consequences for the public health, safety, and welfare. They can be effectively controlled or eliminated with carefully considered attention to design, installation, and use.



Outdoor Pedestrian Space



Dark Sky Lighting



Landscaping Along Parking Lot

Goal: To provide thoughtfully designed outdoor lighting adequate to promote safety and security.

Standards:

4.4.1 All outdoor lighting must be carefully designed to light only the area needed for reasonable levels of safety and security. Seek to eliminate as much outdoor lighting as possible.

4.4.2 Light placement must avoid placing glare on neighboring properties. Every attempt should be made to consider the impacts the additional lighting will have on the surrounding environment

4.4.3 Where appropriate, the use of ground lighting should be considered. Consolidate exterior lighting and minimize the height and projection of the lamps where possible.

4.4.4 Building Lighting

a) Exterior lighting shall be architecturally integrated with the building style, material and colors.

b) Exterior lighting may be mounted flush or projected from the building wall.

c) Neon, bright colors or flashing lights are not acceptable building lighting and are not permitted.

d) Awnings in the Central District shall be lit underneath to ensure adequate light on the sidewalks, but the awnings must not be internally illuminated.

4.4.5 Site Lighting

a) The design of the site lighting should complement the architectural features of the building. Consider the use of the standard City streetlights; however, the use of other lights may be considered by the Committee.

b) Fixture mounting height should be appropriate for the project and the setting. Use of low, bollard-type fixtures, 3 to 4-feet in height, are encouraged as pedestrian area lighting.

c) Raised light pole bases shall be attractively designed and well detailed to be compatible with the overall project. The use of "sonotube" (exposed concrete) bases is discouraged. These foundations, if used, need to be treated in some fashion to cover the exposed concrete.

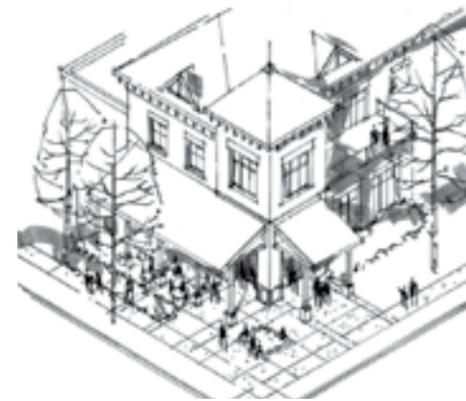
d) The placement of light poles within raised curb planter areas is encouraged; however,



Dark Sky Lighting



New Buildings Complementing Existing Buildings:



conflicts with parking lot trees which can obscure the lighting should be avoided

e) Swivel-mounted luminaires are prohibited.

Note: See Whitefish Zoning Regulations for the Outdoor Lighting Standards. (§11-3-25, WCC)

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All building project designs should be a positive complementary enhancement to the existing architecture, quality of life and character of the Whitefish community and particularly the Old Town District. The positive enhancement should be reflected in the design elements of the project. These design elements are both aesthetic and practical and shall include: 1) scale, character and relationship to surrounding neighborhood; 2) shape and form; and 3) exterior materials; color and texture.

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4.6.1 All buildings must have a scale in size, height and mass that relates to the surrounding buildings and adjacent commercial development.

4.6.2 The size, height and mass must not detract from, conflict with or overwhelm the surrounding neighbors.

4.6.3 Distance from other structures and features will be considered in reviewing scale. For example, the existing railroad station works well at its location; however, if it were on a lot next to the library, it would overwhelm the library.

4.6.4 Building site placement and size should minimize obstruction of views to mountains, Whitefish River, and Whitefish Lake.

4.6.5 Mixed-Use of Non-Residential Development Adjacent to Residential Zones. Buildings must be designed to ensure building massing, height and scale provide sensitive transition to adjoining residential neighborhoods. When abutting a residential zoning district, the project's landscaping plan must include provisions for vegetative screening between the project and the residential property.

New developments adjacent to residential areas must mitigate impact through careful site planning and architectural design. Possible mitigation techniques include, but are not limited to:

- Locating open space and preserving existing vegetation on the site's edges to further separate the building from less intensive uses
- Stepping down the massing of the building along the site's edges
- Limiting the length of or articulating building façade to reflect adjacent residential patterns
- Creative use and ongoing maintenance of landscaping such as berms, buffers, mounds, rockeries, living fences and swales designed to avoid the appearance of a straight line or "wall" of uniform plant material and must be wide enough to accommodate the mature plants

4.6.6 Multiple Building Projects.

a) Buildings must exhibit a general similarity of scale, orientation, and proportion with unified natural and built features. As a rule, taller buildings must be placed toward the interior of the site and stepped back from the street. Buildings must be grouped or sited with other buildings to create distinct outdoor spaces, with distinct pedestrian connections between the buildings, parking and the street. Sites must connect to natural corridors and trail systems and must facilitate pedestrian and bicycle traffic.

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4.7.1 All remote mechanical and electrical equipment (condensers, chillers, intake/exhaust fans, cabinets, etc.) regardless of horsepower, must be enclosed within sound absorptive walls, as permitted by the Building Code. These areas shall be located and screened so the visual and acoustic impacts of these functions are fully contained and out of view. Enclosures shall be reviewed by the Committee where applicable and the acoustic design will be reviewed and approved at the time of plan examination by the Building Department.

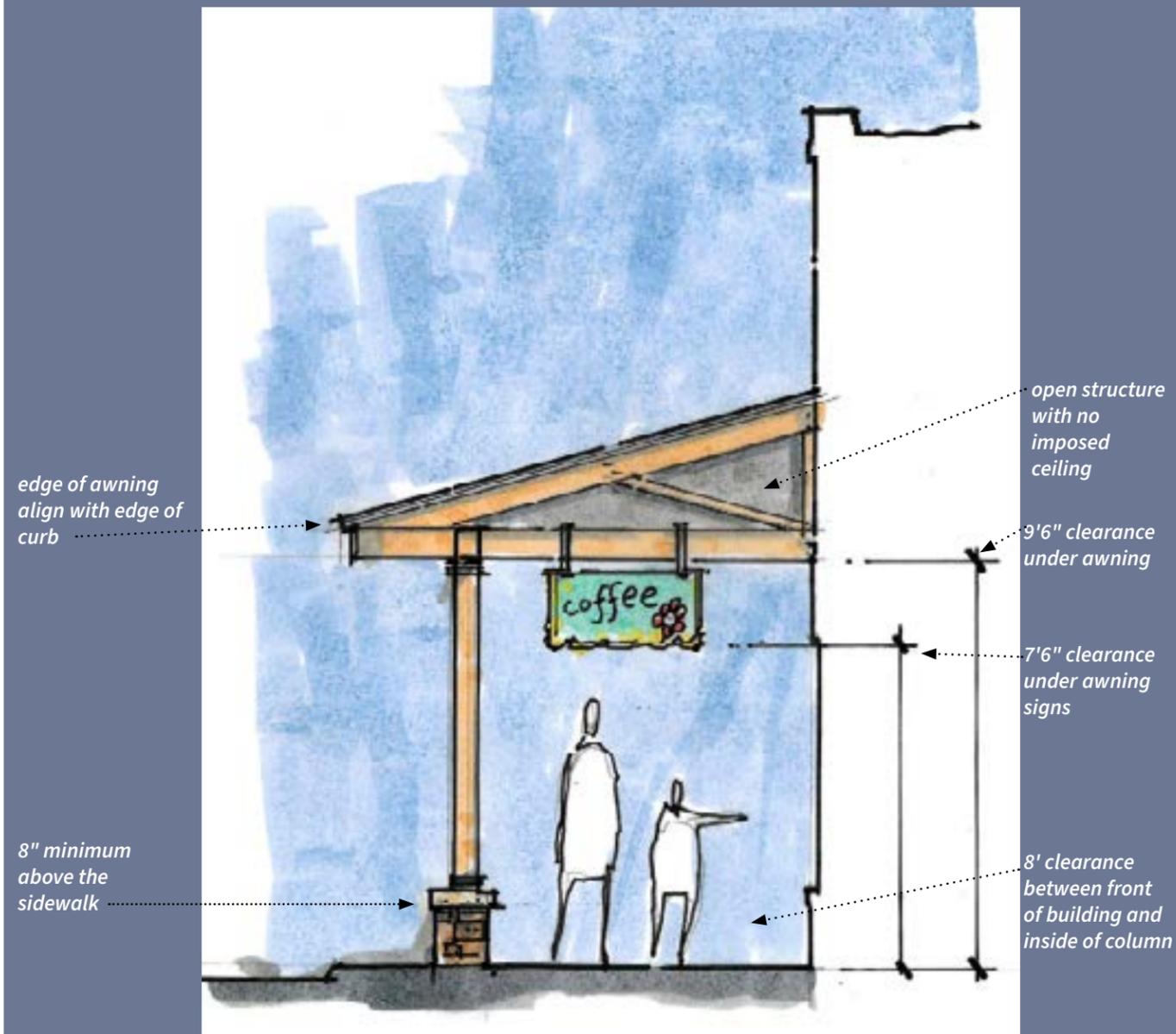


Recessed Entryway



Alternative Awning Style

Awning Design Standards



4.7.2 Awnings over sidewalks are desirable. In neighborhoods where awnings are the norm, new construction shall conform. The objective of awnings is to create a continuous covered shopping arcade; therefore, fixed canopies and awnings are preferred. Such awnings should be constructed of wood, although fabric construction may be acceptable on certain projects. Attention to support post detail is important. The posts must be of high quality and architectural significance (nominal 6x6 or 8x8 typical) or log (6 to 10-inch diameter typical). Posts must be set on concrete or stone piers raised a minimum of 8-inches above sidewalk or grade. Post base and cap

details are required and must be presented for approval. Awnings must have an open structure with no ceilings imposed. Awning heights must be related to adjacent awnings, with a recommended minimum beam height of 9'-6" and recommended minimum clearance for hanging signs of 7'-6". Awning heights must relate to adjacent awnings and must line up with the edge of the curb toward the street, with a typical distance between columns and building walls of 8-feet. Awnings must reflect historic form and shapes.



Recessed Entryway



75% Transparency



Alley Improvement

4.8 Old Town Central District

4.8.1 Regarding shape and form, many of the original downtown façade and rooflines are examples to look toward regarding new construction in this area.

4.8.2 The buildings in this area are one and two story and create a rhythm of 25 to 50-foot width storefronts with rectangular plans unless the applicant can show an acceptable alternative. These features shall be maintained and can be achieved with exterior material application, although actual building heights are regulated per §11-2L-4, WCC.

4.8.3 New construction façades should relate to other buildings in the block with a height that falls within an acceptable historic range. As part of the submittal, the proposed building must be shown within the block of the existing buildings.

4.8.4 Although zero setbacks on the main entry facades are typical and should be maintained, recessed courtyards, porches, stoops and sitting areas will be considered on a case-by-case basis.

4.8.5 Recessed entry doors with large display windows maintain the typical Whitefish main street storefront.

4.8.6 Upper second story windows should be smaller than the main level windows and vertical in shape.

4.8.7 75% of the ground-floor lineal street frontage should have openings for doors and windows. Ground floor glazing should be clear. Non-transparent and reflective or opaque glazing is not permitted.

4.8.8 Ground floor active edges help reinforce the continuity of pedestrian-active building uses, connects street-level activity and accessibility. Buildings should be designed to accommodate single or multiple tenants. Street frontage walls shall include doors and windows or be designed so they can be added when space is converted to active building uses.

4.8.9 Buildings must have roof designs that do not cause snow and drainage problems for themselves or adjacent buildings and sites.

4.8.10 New, and when possible, remodeled buildings should have a minimum of 11-foot floor-to-ceiling heights on the ground floor to promote attractive retail spaces.

4.8.11 Alley façades are encouraged to be developed as an attractive integrated component of the building.

4.8.12 When building on corner lots, consideration should be given to corner entrances with storefront windows that extend along both street facades. Small towers or turrets above corner entrances should also be considered.



Corner Building



South District

4.8.13 Second story balconies are encouraged.

4.8.14 Outdoor courtyards and sitting areas are encouraged.

4.9 Old Town South District

4.9.1 Although zoned for commercial, this area is designed with smaller-scale commercial buildings in keeping with original residential character.

4.9.2 New construction in this area should follow the typical rooflines that include pitched, hipped, gable and gambrel. New roof pitches should stay within the existing roof pitches of 3:12 to 12:12.

4.9.3 Use of dormers is encouraged.

4.9.4 Buildings plans should be primarily rectangular.

4.9.5 Single and 1½ story detached secondary buildings along the alley side are appropriate. Aesthetic consideration must be given to alley side facades.



Railway District

4.9.6 Covered entry porches, stoops and outdoor courtyards/sitting areas are encouraged.

4.10 Old Town Railway District

4.10.1 Buildings in this district must maintain the historical residential character of the area. Building roofs should pitch to the sides and not to the front or rear. This general form should remain.

4.10.2 Lots fronting on the Highway must be designed to frame the street and may not necessarily have a primarily residential character. These buildings are the gateway into the downtown and may have a traditional downtown roof design with flat roofs and parapets.

4.10.3 Front porches and stoops are required at entrances.

4.10.4 Detached garages and storage buildings of 1 and 1½ story with pitched roofs are encouraged.

4.10.5 Outdoor courtyards and sitting areas are encouraged.

4.10.6 Bay windows and flower boxes are encouraged.

4.11 Exterior Materials

4.11.1 Exterior finish building materials should be of a nature in color and texture complementary with the structures within each building district. They shall not clash, detract or conflict with adjacent architecture.

4.11.2 Acceptable exterior cladding materials should include properly scaled brick, 1x4, 1x6 or 1x8 horizontal natural wood siding, sawn shingles 1x6 or 1x8 vertical channel natural siding, 1x8 to 1x12 vertical board and batten natural wood siding with 1x2 batts, natural stone veneers, and stucco with wood trim. Other fire resistant and ecological friendly materials may be considered.

4.11.3 Wood shingles, vertical wood siding and boards w/ batts are acceptable in gables and upper portions of walls.

4.11.4 Exposed plain concrete block, and vinyl cladding are not allowed. Metal siding, Exterior Insulation Finish System (EIFS), composite siding and stone panels are discouraged but may be used at the discretion of the Committee.

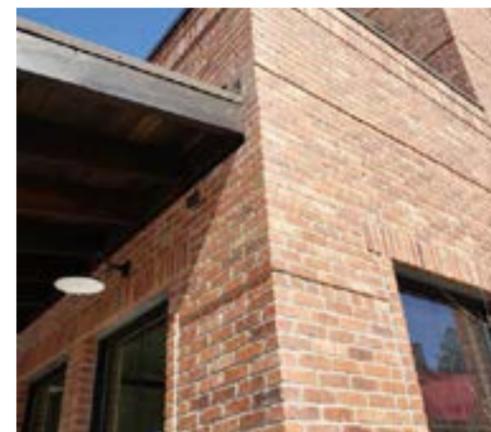
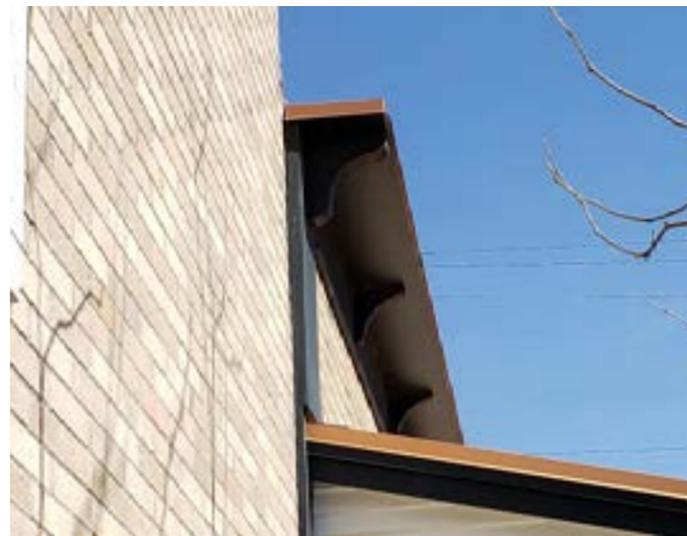
4.11.5 Acceptable roofing materials shall be asphalt composition shingles, metal, or appropriate flat roofing. Other technologically advanced products such as solar roof tiles are encouraged.

4.11.6 Roofing shall be of colors that complement neighboring structures.

4.11.7 Mirrored and tinted glazing is not allowed. Low-E glass is acceptable. Vision glass on building sides facing street is required.

4.11.8 Exterior classic details and elements such as brackets, cornices, window trim, and wall corner boards are encouraged.

4.11.9 Higher quality and easily maintained materials are strongly recommended.



Central Avenue c. 1962
Provided by the Stumptown Historical Society

RESORT COMMUNITY BUSINESS DISTRICT

Chapter 5

The Resort Community Business District is comprised of two corridors. One corridor is located along Wisconsin Avenue/East Lakeshore Drive from the viaduct to the north and the other is West 2nd Street from the Whitefish River to the west. Both corridors are located along state highways and have adopted corridor plans – Wisconsin Avenue Corridor Plan (2018) and the Highway 93 West Corridor Plan (2015). This District is for those areas zoned WB-1: Limited Business District, Limited Resort Business District: WRB-1, General Resort Business District: WRB-2 or with a multi-family designation that could be developed into offices (WR-3 and WR-4) along these two corridors. The character of these areas include small scale commercial and professional office uses interspersed with a variety of residential types. The Wisconsin Avenue corridor is evolving into providing resort-oriented and neighborhood services with multi-family while the West 2nd Street corridor is evolving into an area with small professional offices and multi-family. In addition to these standards, the Mixed-Use and Non-Residential Building Development Standards (§11-3-43, WCC) apply.

Goals:

- Review the existing examples of successful residential conversions.
- Developments along the Wisconsin Avenue and West 2nd Street corridors have historically developed with larger setbacks from the

highway. In order to preserve a landscaped entrance into the downtown core and retain a residential feel, development should continue this pattern by setting new construction as far back as possible on the lot with landscaping and/or courtyards in front rather than parking.

- Orient buildings toward public streets.
- Include design of small pocket parks to encourage employees and the community to embrace the outdoor environment.
- When constructing new parking consider the use of a landscape buffer to minimize impact on neighboring properties.
- When selecting a building envelope consider its relationship to the lot with respect to scale. Consider staggering the structures in the new development.
- Protect the neighborhoods' existing character by using existing shapes, forms, and structural elements. Use the existing character of the neighborhood as a context for all new development. Do not use new construction to redefine the feeling and presence of the neighborhood.
- Infill and adaptive reuse projects are encouraged and shall complement the characteristics of the existing buildings built in the District.

The Resort Community Business Districts will develop as areas with smaller scale buildings in order to be good neighbors to residential areas located immediately adjacent to these corridors. Parking areas will be sited in order to not conflict with surrounding residential neighborhoods. Sites will be designed to accommodate bicyclists and pedestrians. Ample landscaping will be installed – especially to screen undesirable qualities of commercial uses from adjoining residential areas.



5.1 Site Design Standards

5.1.1 Buildings must be arranged on the site so their orientation frames, encloses or otherwise gives prominence to a pedestrian corridor, an outdoor gathering space with outdoor seating, a "main street" pedestrian or vehicle access corridor within the site or the corners of the street intersections or entry points into the development.

5.1.2 Buildings on corner lots should be oriented toward the primary intersection and the primary and secondary street frontage, while parking and auto access must be located away from the primary intersection corners. The use of corner entrances, plazas, signage and/or landscaping is encouraged to accentuate corner sites.

5.1.3 Where buildings are separated from the public sidewalk along the primary street frontage, the space should contain public and pedestrian amenities. Buildings may be separated from the sidewalk by plazas, landscaping, benches, bicycle racks, trash receptacles and other pedestrian amenities.

5.1.4 The layout of the site must consider pedestrians and bicyclists, as well as automobile traffic. Pedestrian access from adjacent sidewalks must have priority over vehicular traffic. Consider protection from the weather.

Consider how a pedestrian or a bicyclist would access the proposed site. Mixing nonmotorized transportation in the same spaces as automobile

traffic can be a very unpleasant experience for both pedestrians and bicyclists. Consider alternate and/or separate routes into the development that clearly define the pedestrian area versus areas for vehicles.

5.1.5 Minimize the dominance of parking surfaces and structures. Use green spaces to soften the "sea of asphalt" which often dominates commercial "strip" development. A close review of the City's landscaping ordinance is highly recommended. Consider building orientation and its relationship to the entire site.

5.1.6 All new development must invite pedestrian access. Thoughtful site layout and the use of "green open space" should be included in all site planning. The front door/main entrance to a development must be easy to identify to the pedestrian. This may include providing a direct access from a public sidewalk to the front entrance.

5.1.7 Make site more appealing to pedestrian and bicycle traffic and use. Thoughtful application of design principles and space planning can make a site more pleasant to view and a better, safer, place to be. The use of courtyards, decks, traditional front porches and bicycle amenities are design tools that can accomplish this goal. Consider protection from the weather.

5.1.8 Fencing should be of a natural material. The use of chain link and vinyl should be avoided but may be considered by the Committee.



Bike Parking



Courtyard

5.1.9 Site design must consider the seasonal nature of the Whitefish climate allowing for summer enjoyment and snow storage in the winter. Also, consider southern exposure, energy costs and impact of shadows on adjacent properties. Apply design principles that maximize the seasonal nature of our climate.

5.1.10 Promote safe ingress and egress. Consolidate site access to the public rights-of-way to reduce congestion and the need for endless stoplights. The City seeks to promote shared access where possible. Provide cross-property easements to share driveways and reduce the need for additional curb cuts, when feasible. (§11-3-43E(5), WCC)

5.2 Landscaping Design Standards

Goal: Thoughtfully designed landscaping will contribute to a pleasing character for the development and Whitefish. Landscaping should not be used just as a visual screen but should also encourage a sense of place and connection to the environment.

Standards:

5.2.1 Preserve and maintain mature trees and existing vegetation as much as practical.

5.2.2 A thoughtful selection of appropriate vegetation and landscaping materials. The designer should carefully select hardy vegetation and materials to survive our harsh climate, while also accentuating the natural beauty of our community. Use native, drought-resistant species and plants requiring low amounts of water, chemicals and fertilizers.

5.2.3 Landscaped areas must be planned as an

integral part of the project, rather than simply located in the left-over spaces on the site.

5.2.4 Landscaping should complement the architecture of the project and adjacent buildings enhancing the indoor-outdoor relationship.

5.2.5 Landscaping (along with the location of the building) should soften the appearance of the site as seen from the roadway.

5.2.6 It is beneficial and attractive to incorporate outside seating areas in the landscaping plan.

5.2.7 In a landscape plan, attention should be paid to the plant material's color and texture to create unity on the site. Simple masses of material with one predominant species provide unity, while accent plantings provide attractive contrasts.

5.2.8 Species should be planted which are suitable to the space they will occupy when they have achieved their full growth. This includes understanding the growth of species root mass and their effect on sidewalks, utilities and building foundations.

5.2.9 Plants with multi-season interest provide color and interest throughout the year.

5.2.10 Plazas and malls should be planted to reflect an informal place suited to pedestrian scale. Variety and color are encouraged.

5.2.11 Landscaping and earthen berms or sight obscuring fencings must be used to screen equipment, refuse areas and storage areas. It is better to mass plant material in groups rather than locate them in a straight line which tends to look unnatural, except when required to provide



Parking Behind Commercial Building

a landscape buffer as defined in the Landscape Requirements. (§11-4, WCC)

5.2.12 Landscaping depicted on renderings/perspectives should be a realistic interpretation of what will be installed at the time of planting.

5.2.13 Provide uniform planting of required street trees.

5.2.14 The final landscape, irrigation and tree preservation techniques shall be reviewed and approved by staff prior to issuance of a building permit.

Note: See Whitefish Zoning Regulations for the Landscaping requirements. (§11-4, WCC)

5.3 Off-Street Parking Design Standards

Goal: Parking lots in Whitefish will be designed as a necessary appurtenance of a site and not the primary visual emphasis.

Standards:

5.3.1 The use of large parking lots as the primary feature at the entrances of businesses is strongly discouraged.

5.3.2 When possible locate parking to the rear or side of buildings, unless it is adjacent to a residential use or residential district. If parking is proposed adjacent to residential, it needs to be screened using fencing and landscaping. The siting of parking lots is regulated through the off-street parking chapter of the zoning.

5.3.3 Parking lots shall be designed to consider all users and not only automobiles. The development must minimize the impact of parking on the building's relationship to the street and pedestrian-oriented character and character of the neighborhood. Secure, well-lit and convenient bicycle parking and storage must be provided.

5.3.4 Provide cross-property easements to share driveways and reduce the need for additional curb cuts, when feasible. At a minimum, a pedestrian connection between parking lots between developments should be explored. Parking lot design should consider a southern aspect to reduce the accumulation of ice during the winter and allow for natural melting.

5.3.5 Parking areas shall be designed to minimize the lot's visual impact, thus creating as much as possible an architectural form instead of a wasteland.

5.3.6 Parking structures built below grade are desirable because they less obvious to passers-by, and are easier to screen with landscaping.

5.3.7 Soften large parking areas with landscaping, earthen berms and pedestrian friendly features.

5.3.8 The amount of unrelieved pavement on parking lots shall be limited by methods such as the use of landscaping, contrasting colors, and pathways of alternate paving material.

5.3.9 Landscaping treatments of parking lots should include trees both on the perimeter and interior of the lots.

5.3.10 Trees in parking lots shall be protected from vehicle damage by concrete curbing or other effective means and must provide an adequate root zone uncompacted by vehicular traffic. In high traffic areas, the use of subsurface tree root protection is recommended.

Note: See Whitefish Zoning Regulations for the Off-Street Parking requirements. (§11-6, WCC)

5.4 Exterior Lighting Design Standards

Although there are many needs for lighting in our built environments, obtrusive aspects of lighting often extend well beyond the boundaries of the area in which the lighting

is installed and intended for use. These obtrusive aspects, such as glare, light trespass, energy waste and sky glow, can have serious consequences for the public health, safety, and welfare. They can be effectively controlled or eliminated with carefully considered attention to design, installation, and use.

Goal: To provide thoughtfully designed outdoor lighting adequate to promote safety and security.

Standards:

5.4.1 All outdoor lighting must be carefully designed to light only the area needed for reasonable levels of safety and security. Seek to eliminate as much outdoor lighting as possible.

5.4.2 Light placement must avoid placing glare on neighboring properties. Every attempt should be made to consider the impacts the additional lighting will have on the surrounding environment

5.4.3 Where appropriate, the use of ground lighting should be considered. Consolidate exterior lighting and minimize the height and projection of the lamps where possible.

5.4.4 Building Lighting.

a) Exterior lighting shall be architecturally integrated with the building style, material and colors.

b) Exterior lighting may be mounted flush or projected from the building wall.

c) Neon, bright colors or flashing lights are not acceptable building lighting and are not permitted.

5.4.5 Site Lighting.

a) The design of the site lighting should

complement the architectural features of the building. Consider the use of the standard City streetlights; however, the use of other lights may be considered by the Committee.

b) Fixture mounting height should be appropriate for the project and the setting. Use of low, bollard-type fixtures, 3 to 4-feet in height, are encouraged as pedestrian area lighting.

c) Raised light pole bases shall be attractively designed and well detailed to be compatible with the overall project. The use of "sonotube" (exposed concrete) bases is discouraged. These foundations, if used, need to be treated in some fashion to cover the exposed concrete.

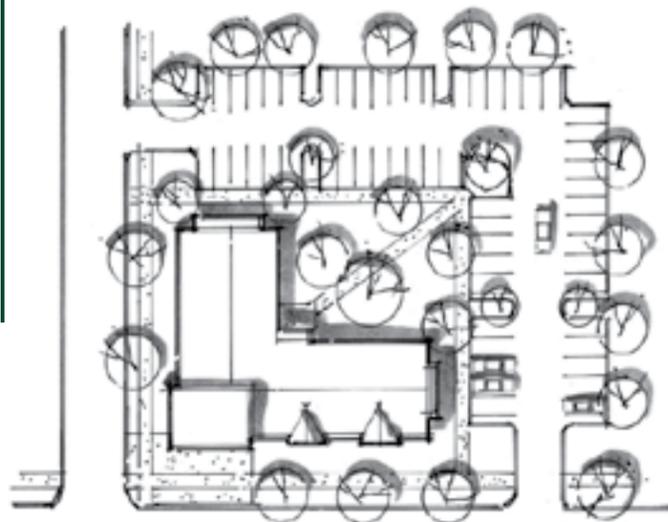
d) The placement of light poles within raised curb planter areas is encouraged; however, conflicts with parking lot trees which can obscure the lighting should be avoided

e) Swivel-mounted luminaires are prohibited.

Note: See Whitefish Zoning Regulations for the Outdoor Lighting Standards. (§11-3-25, WCC)

5.5 Building Design Standards

All building project designs should be a positive complementary enhancement to the existing architecture, quality of life and character of the Whitefish community and particularly the Resort Community Business District. The positive enhancement should be reflected in the design elements of the project. These



Parking to Side & Rear of Project



Dark Sky Lighting



Residential Scale Commercial Building



Properly Scaled Commercial Building

design elements are both aesthetic and practical and shall include: 1) scale, character and relationship to surrounding neighborhood; 2) shape and form; and 3) exterior materials. Also, businesses considered formula retail (also known as franchise businesses) may not use their traditional building design or color scheme. See Section 5.8 for these standards.

5.6 Scale and Relationship to Surroundings

5.6.1 All buildings must have a scale in size, height and mass that relates to the surrounding buildings and adjacent commercial development and neighborhood.

5.6.2 The size, height and mass must not detract from, conflict with or overwhelm the surrounding neighbors. Taller buildings must be stepped down when neighboring less high buildings.

5.6.3 Distance from other structures and features will be considered in reviewing scale.

5.6.4 Building site placement and size should minimize obstruction of views to mountains, Whitefish River, and Whitefish Lake.

5.6.5 Mixed-Use of Non-Residential Development Adjacent to Residential Zones. Buildings must be designed to ensure building massing, height and scale provide sensitive transition to adjoining residential neighborhoods. When abutting a residential zoning district, the project's landscaping plan must include

provisions for vegetative screening between the project and the residential property.

New developments adjacent to residential areas must mitigate impact through careful site planning and architectural design. Possible mitigation techniques include, but are not limited to:

- Locating open space and preserving existing vegetation on the site's edges to further separate the building from less intensive uses.
- Stepping down the massing of the building along the site's edges
- Limiting the length of or articulating building façade to reflect adjacent residential patterns;
- Creative use and ongoing maintenance of landscaping such as berms, buffers, mounds, rockeries, living fences and swales designed to avoid the appearance of a straight line or "wall" of uniform plant material and must be wide enough to accommodate the mature plants.

5.6.6 Multiple Building Projects

a) Buildings must exhibit a general similarity of scale, orientation, and proportion with unified natural and built features. As a rule, taller buildings must be placed toward the interior of the site and stepped back from the street. Buildings must be grouped or sited with other buildings to create distinct outdoor spaces, with distinct pedestrian connections between the buildings, parking and the street. Sites must connect to natural corridors and trail systems and must facilitate pedestrian and bicycle traffic.

b) Projects with multiple buildings to be built in phases on one site must include design information for each building. Initial buildings must meet all the requirements of the Architectural Review Standards. Information on buildings to be built in the future must include an entire site development narrative with a site plan describing buildings, locations, access for parking and pedestrians, general landscaping and concept of building sizes, shapes, materials and colors.

5.7 Shape and Form

All buildings shall have a shape and form regarding rooflines and massing that relates to the surrounding buildings and neighborhood. Rooflines should be pitched, pitched with false fronts, flat, flat with false fronts, and shed with false fronts, hipped, gable or gambrel. Overhanging eaves are encouraged. Flat roofs shall have parapets.

Mechanical Equipment Screening



5.7.1 All remote mechanical and electrical equipment (condensers, chillers, intake/exhaust fans, cabinets, etc.) regardless of horsepower, must be enclosed within sound absorptive walls as permitted by the Building Code. These areas shall be located and screened so the visual and acoustic impacts of these functions are fully contained and out of view. Enclosures shall be reviewed by the Committee where applicable and the acoustic design will be reviewed and approved at the time of plan examination by the Building Department.

5.7.2 Although business buildings are permitted in this area, it is historically a residential neighborhood and new construction in this district must respect the residential character in style, scale and form.

5.7.3 New construction in this district should follow the typical rooflines, which include pitched, hipped, gable, and sheds with overhangs or false fronts. New roof pitches should stay within the existing roof pitches of 3:12 to 12:12.

5.7.4 Large massive roofs should be avoided. When large floor plans are required, roofs must be broken up with elements such as dormers, hips and pitched entries.

5.7.5 Rear façades of buildings facing alleys or streets must be given aesthetic considerations.

5.7.6 Front porches and covered entries are recommended.

5.7.7 Courtyard and sitting areas away from the street side are encouraged.

5.7.8 Expansive blank walls are not allowed.

5.7.9 Large display windows facing a main street at the first story, as in the downtown area, are allowed. All other windows on sidewall and upper stories should be vertical in orientation.

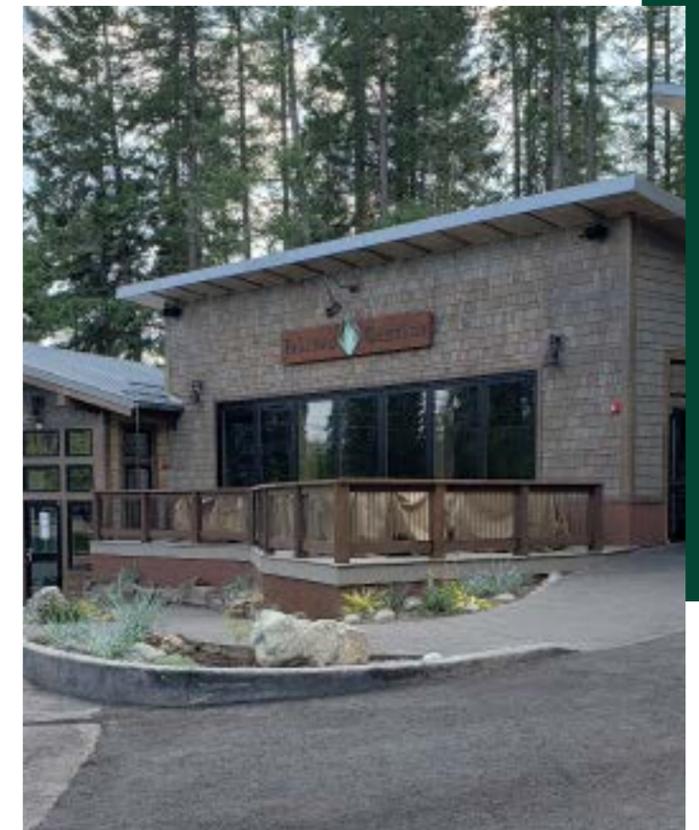
5.8 Exterior Materials

5.8.1 Exterior finish building materials should be of a nature in color and texture complementary with the structures within each building district. They shall not clash, detract or conflict with adjacent architecture.

5.8.2 Acceptable exterior cladding materials should include properly scaled brick, 1x4, 1x6 or 1x8 horizontal natural wood siding, sawn shingles 1x6 or 1x8 vertical channel natural siding, 1x8 to 1x12 vertical board and batten natural wood siding with 1x2 batts, natural stone veneers, and stucco with wood trim. Other fire resistant and ecological friendly materials may be considered.

5.8.3 Wood shingles, vertical wood siding and boards w/ batts are acceptable in gables and upper portions of walls.

5.8.4 Exposed plain concrete block, and vinyl cladding are not allowed. Metal siding, Exterior Insulation Finish System (EIFS), composite siding and stone panels are discouraged but may be used at the discretion of the Committee. (Exception: residential buildings of two or more



Exterior Customer Seating



Residential Scale Commercial Buildings



units when located in a strictly residential district may utilize composite sidings such as fiber cement if compatible with neighboring structures.)

5.8.5 Acceptable roofing materials shall be asphalt composition shingles, metal, or appropriate flat roofing. Other technologically advanced products such as solar roof tiles are encouraged.

5.8.6. Roofing shall be of colors that complement neighboring structures.

5.8.7. Mirrored and tinted glazing is not allowed. Low-E glass is acceptable. Vision glass on building sides facing streets is encouraged.

5.8.8. Exterior classic details and elements such as brackets, cornices, window trim, and wall corner boards are encouraged.

5.8.9. Higher quality and easily maintained materials are strongly recommended.

5.9 Formula Business Design Standards

Commercial uses meeting the zoning definition of “formula business” are expected to minimize generic and corporate images, and express the architectural elements and forms that best express the values and heritage of the community.

5.9.1. Formula business must not use standard corporate building shapes, forms and rooflines.

5.9.2. Formula business color schemes are not acceptable.





Properly Scaled Residential Buildings



RESIDENTIAL DISTRICT

Chapter 6

The Residential Standards apply to two-unit townhouses, triplexes, fourplexes and multi-family structures. The Residential District is all areas within the city limits without another design district designation. In addition, a residential use within one of the other design districts requires compliance with these standards along with the applicable design district standards.

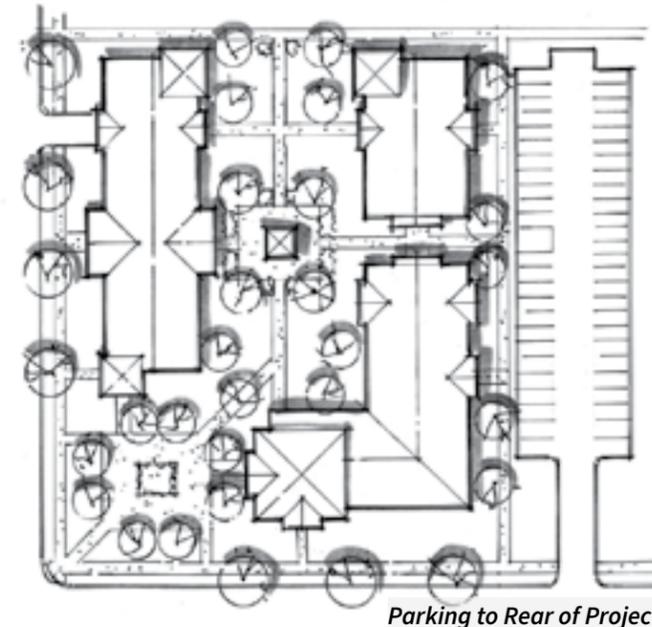
In addition to these standards, the Multi-Family Development Standards (§11-3-42, WCC) apply.

Goals:

- Consider vertical relief when laying out building footprints on the lot.

- Maximize the use of parking to the rear of the lot and the use of alleys for entrances.
- Incorporate usable on-site pedestrian spaces.
- Reduce the prominence of the garage and parking pavement in neighborhood design. Locate garages behind the primary structure on the lot or setback from the front foundation line of the structure. Parking pavement should not extend to the front door. Use alleys wherever possible for the primary entrances to on-site parking. If this is not feasible, employ design standards described in Section 6.8.

Multi-family development in Whitefish will be designed to suit the neighborhood where it is located, relate to the street and surroundings and meet the needs of the residents.



Parking to Rear of Project

6.1 Site Design Standards

6.1.1 The layout of the site must consider pedestrians and bicyclists, as well as automobile traffic. Pedestrian access from adjacent sidewalk must have priority over vehicular traffic. Provide each building with direct and distinct pedestrian access from the street fronting the building and the parking areas. A bicycle parking area must be provided. Consider protection from the weather.

Consider how a pedestrian or a bicyclist would access the proposed site. Mixing nonmotorized transportation in the same spaces as automobile traffic can be a very unpleasant, and potentially dangerous, experience for pedestrians and bicyclists. Consider alternate and/or separate routes into the development that clearly define the pedestrian area versus areas for vehicles.

6.1.2 Minimize the dominance of parking surfaces and structures. Use green spaces to soften the “sea of asphalt”. A close review of the City’s landscaping ordinance is highly recommended. Consider building orientation and its relationship to the entire site.

6.1.3 All new development must invite pedestrian access and be oriented to public streets. Design alternate and/or separate routes into the development clearly defining bicycle and pedestrian areas versus areas for vehicles.

Thoughtful site layout and the use of “green open space” should be included in all site

Residential Fencing



planning. The front door/main entrance to a development must be easy to identify to the pedestrian. This may include providing a direct access from a public sidewalk or street to the front entrance.

6.1.4 Make site more appealing to pedestrian and bicycle traffic and use. Thoughtful application of design principles and space planning can make a site more pleasant to view and a better, safer place to be. The use of courtyards, decks, traditional front porches and bicycle amenities are design tools that can accomplish this goal. Consider protection from the weather and provide safe, visible well-lit locations. Where feasible, designs shall encourage front yard and outdoor spaces for barbecuing, sitting, gardening and playing.

6.1.5 Fencing should be of a natural material. The use of chain link and vinyl should be avoided but may be considered by the Committee.

6.1.6 Site design must consider the seasonal nature of the Whitefish climate allowing for summer enjoyment and snow storage in the winter. Also, consider southern exposure, energy costs and impact of shadows on adjacent properties. Apply design principles that maximize the seasonal nature of our climate.

6.1.7 Promote safe ingress and egress. Consolidate site access to public rights-of-way to reduce congestion and the need for endless stoplights. The City seeks to promote shared access where possible. Provide for cross-property easement to share driveway and

reduce the need for additional curb-cuts when feasible. (§11-3-42C, WCC)

6.1.8 Buildings shall be oriented for privacy, both within the project and to the neighborhood.

6.2 Landscaping Design Standards

Goal: Thoughtfully designed landscaping will contribute to a pleasing character for the development and Whitefish. Landscaping should not be used just as a visual screen but should also encourage a sense of place and connection to the environment.

Standards:

6.2.1 Landscaped areas must be planned as an integral part of the project, rather than simply located in the left-over spaces on the site.

6.2.2 A thoughtful selection of appropriate vegetation and landscaping materials. The designer should carefully select hardy vegetation and materials that can survive our harsh climate; these plants should also accentuate the natural beauty of our community. Use native, drought-resistant species and plants requiring low amounts of water, chemicals and fertilizers. Designs should have the ability to be maintained reasonably weed free.

6.2.3 Preserve and maintain mature trees and existing vegetation as much as practical.

6.2.4 Landscaping should complement the architecture of the project and adjacent buildings.

6.2.5 Landscaping (along with the location of the building) should soften the appearance of the site as seen from the roadway.

6.2.6 It is beneficial and attractive to incorporate

outside areas in the landscaping plan such as trellises, low planters, walls, seating, porches or balconies.

6.2.7 Consider wildflower gardens, rain gardens or vegetable gardens with defined planting beds and features such as rocks or sculptures in lieu of front lawns. Alternatives to traditional lawn grasses include low mow Fescue grass mixes, native and non-native perennial ground-covers such as thyme, chamomile, sea thrift, Scotch moss, creeping potentilla, sedums and other perennial ground-covers; combination of ground-covers will increase resiliency to drought and pests.

6.2.8 In a landscape plan, attention should be paid to the plant material's color and texture to create unity on the site. Simple masses of material with one predominant species provide unity, while accent plantings provide attractive contrasts.

6.2.9 Species should be planted which are suitable to the space they will occupy when they have achieved their full growth. This includes understanding the growth of species root mass and their effect on sidewalks, utilities and building foundations.

6.2.10 Plants with multi-season interest provide color and interest throughout the year.

6.2.11 Plazas should be planted to reflect an informal place suited to pedestrian scale. Variety and color are encouraged.

6.2.12 Landscaping and earthen berms or sight obscuring fences must be used to screen equipment, refuse areas and storage areas. It is better to mass plant material in groups rather than locate them in a straight line which tends to look unnatural, except when required to provide a landscape buffer as defined in the Landscape Requirements. (§11-4, WCC)

6.2.13 Landscaping plans submitted for review and approval should be consistent with materials presented for Architectural Review approval. This includes location and size of planting beds and specific tree and shrub species.

6.2.14 Provide uniform planting of required street trees.

6.2.15 The final landscape, irrigation and tree preservation techniques shall be reviewed and approved by staff prior to issuance of a building permit.

Note: See Whitefish Zoning Regulations for the Landscaping requirements. (§11-4, WCC)

6.3 Off-Street Parking Design Standards

Goal: Parking in the Residential District will be designed as a necessary appurtenance of a site and not the primary visual emphasis. The effect of driveways and parking lots on the public and neighboring properties must be minimized by designing, locating and screening parking lots, carports and garages in a way to create few interruptions on the street, sidewalk and building facades.

Standards:

6.3.1 Parking areas shall be designed to minimize the lot's visual impact, thus creating an architectural form instead of a wasteland as much as possible.

6.3.2 Parking lots shall be designed to consider all users and not only automobiles. Secure, well-lit and convenient bicycle parking and storage must be provided.

6.3.3 Parking must be distributed throughout the site with a preference for parking to the side or rear of the building or under the building. Such parking should be accessed from alley-type driveways. Shared driveways between

adjacent uses is preferred. Parking lot design should consider a southern aspect to reduce the accumulation of ice during the winter and allow for natural melting.

6.3.4 Parking structures built below grade are desirable because they are less obvious to passers-by, and are easier to screen with landscaping.

6.3.5 Soften large parking areas with landscaping, earthen berms and pedestrian friendly features.

6.3.6 The amount of unrelieved pavement on parking lots shall be limited by methods such as the use of landscaping, contrasting colors, and pathways of alternate paving material.

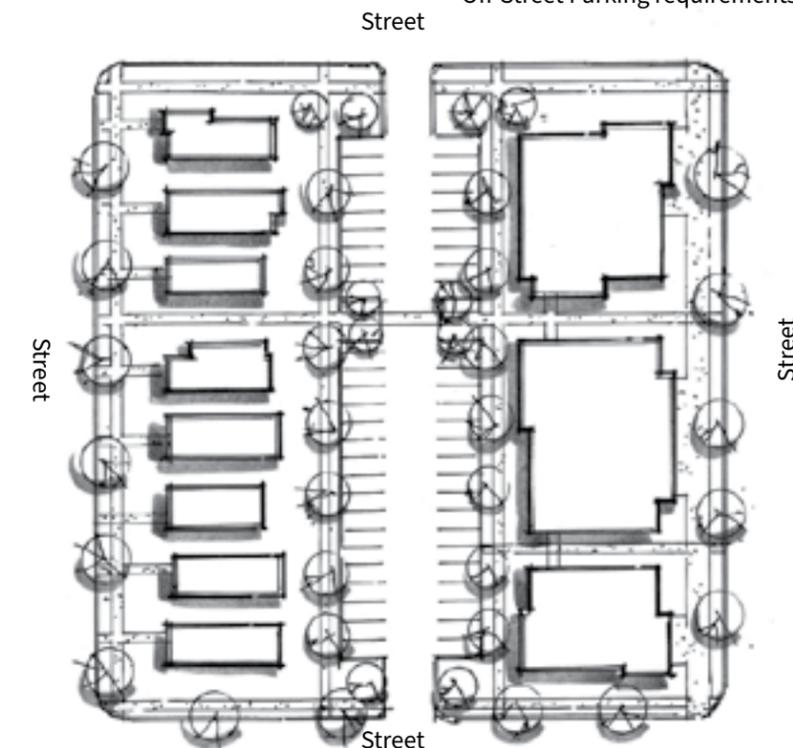
6.3.7 Parking lots shall be designed with adequate landscaping to avoid the appearance of a sea of asphalt.

6.3.8 Landscaping treatments of parking lots should include trees both on the perimeter and interior of the lots.

6.3.9 Trees in parking lots shall be protected from vehicle damage by concrete curbing or other effective means and must provide an adequate root zone uncompacted by vehicular traffic. In high traffic areas, the use of subsurface tree root protection is recommended.

Note: See Whitefish Zoning Regulations for the Off-Street Parking requirements. (§11-6, WCC)

Residential Fencing



Multi-Family Oriented Toward Street with Parking Behind

RESIDENTIAL SITE PLANS

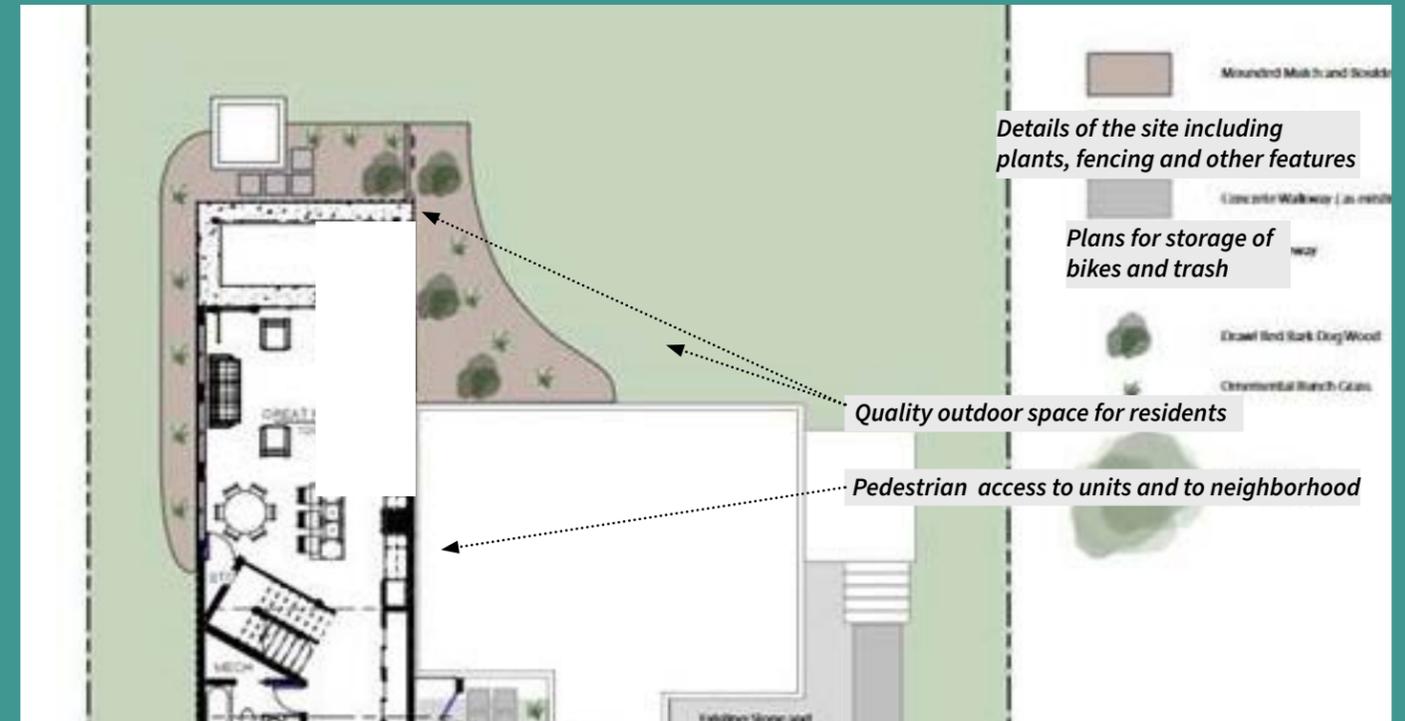
A well-designed site plan will indicate careful consideration of:

- ✓ how a resident will interact with the outdoors
- ✓ provide quality outdoor space - whether that be a front porch with a front yard or a common larger open space area or a courtyard available for all residents
- ✓ include pedestrian and bicycle connections to the public sidewalk and/or trail system
- ✓ bicycle parking/storage
- ✓ incorporation of mature long-lived trees, where possible

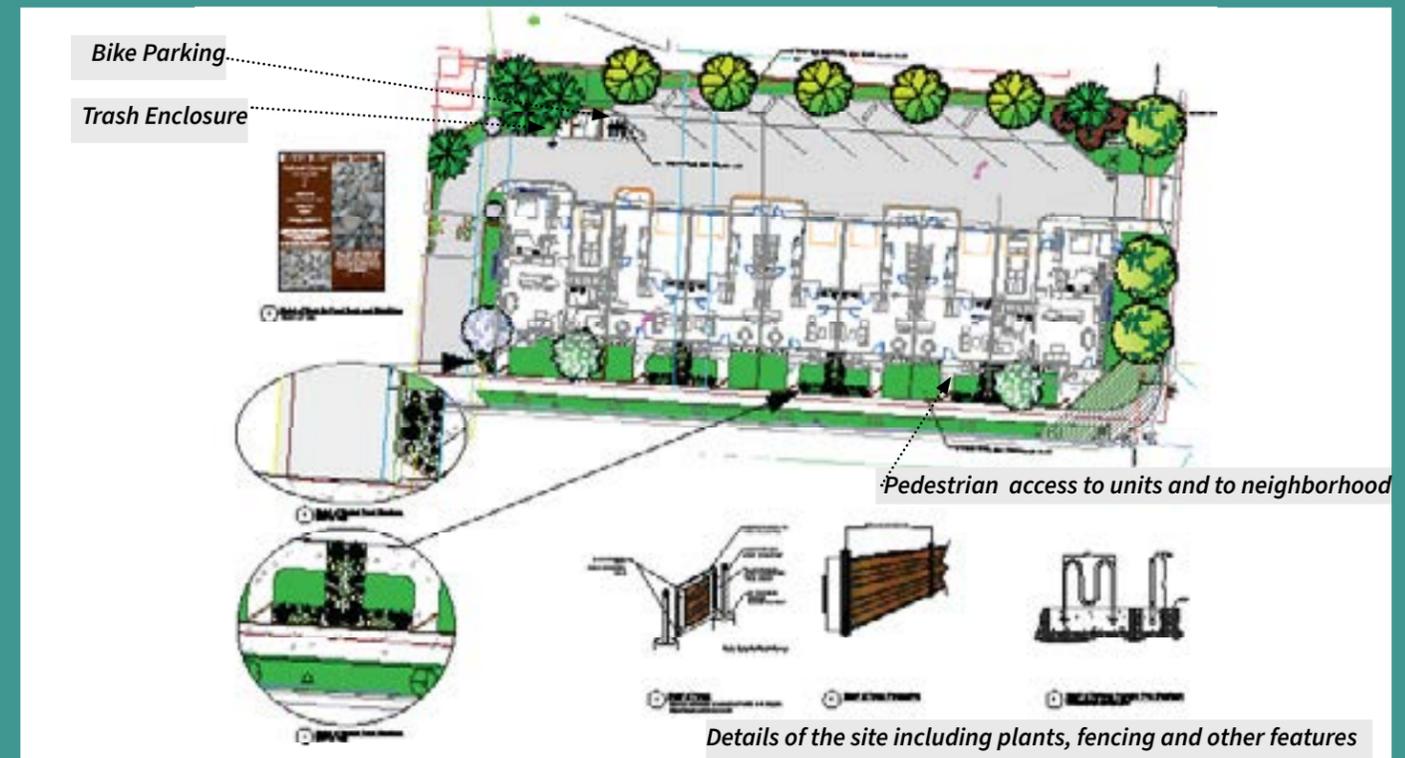
At a minimum, the site plan must be drawn to scale and show the following:

- ✓ detail showing proposed use
- ✓ property lines
- ✓ existing and proposed buildings
- ✓ traffic circulation
- ✓ driveways
- ✓ parking
- ✓ landscaping concept plan including lighting, plantings, hard surfaces, sidewalks, patios and other site features
- ✓ fencing
- ✓ signage
- ✓ topographic features

SMALL PROJECTS



LARGE PROJECTS



6.4 Exterior Lighting Design Standards

Although there are many needs for lighting in our built environments, obtrusive aspects of lighting often extend well beyond the boundaries of the area in which the lighting is installed and intended for use. These obtrusive aspects, such as glare, light trespass, energy waste and sky glow, can have serious consequences for the public health, safety, and welfare. They can be effectively controlled or eliminated with carefully considered attention to design, installation, and use.

Goal: To provide thoughtfully designed outdoor lighting adequate to promote safety and security.

Standards:

6.4.1 All outdoor lighting must be carefully designed to light only the area needed for reasonable levels of safety and security. Seek to eliminate as much outdoor lighting as possible.

6.4.2 Light placement must avoid placing glare on neighboring properties. Every attempt should be made to consider the impacts the additional lighting will have on the surrounding environment

6.4.3 Where appropriate, the use of ground lighting should be considered. Consolidate exterior lighting and minimize the height and projection of the lamps where possible.

6.4.4 Building Lighting

a) Exterior lighting shall be architecturally integrated with the building style, material and colors.

b) Exterior lighting may be mounted flush or projected from the building wall.

c) Neon, bright colors or flashing lights are not acceptable building lighting and are not permitted.

6.4.5 Site Lighting

a) The design of the site lighting should complement the architectural features of the building. Consider the use of City standard streetlights; however, the use of other lights may be considered by the Committee.

b) Fixture mounting height should be appropriate for the project and the setting. Use of low, bollard-type fixtures, 3 to 4-feet in height,

are encouraged as pedestrian area lighting.

c) Raised light pole bases shall be attractively designed and well detailed to be compatible with the overall project. The use of “sonotube” (exposed concrete) bases is discouraged. These foundations, if used, need to be treated in some fashion to cover the exposed concrete.

d) The placement of light poles within raised curb planter areas is encouraged; however, conflicts with parking lot trees which can obscure the lighting should be avoided.

e) Swivel-mounted luminaires are prohibited.

Note: See Whitefish Zoning Regulations for the Outdoor Lighting Standards. (§11-3-25, WCC)

6.5 Building Design Standards

All building project designs should be a positive complementary enhancement to the existing architecture, quality of life and character of the Whitefish community and particularly the Residential District. The positive enhancement should be reflected in the design elements of the project. These design elements are both aesthetic and practical and shall include:

1) scale, character and relationship to surrounding neighborhood; 2) shape and form; and 3) exterior materials. Projects with more than one two-unit townhouse, triplex, fourplex or multi-family structure are required to complement the existing neighborhood in mass and scale and have a varying exterior



Multi-Family Stepped Down to Existing Single Family Homes

appearance. These standards can be found under Section 6.7.2. In addition, two-unit townhouse, triplex, fourplex and multi-family structures with garages designed forward of the main foundation of the structure generally lack connectivity to the neighborhood and street. This design is prohibited in Whitefish unless it is unavoidable, then the standards in Section 6.8 must be met.

6.6 Scale and Relationship to Surrounding Neighborhood

6.6.1 All buildings must have a scale in size, height and mass that is compatible, complementary to and relates to the immediate surrounding buildings and neighborhood.

6.6.2 The size, height and mass must not detract from, conflict with or overwhelm the surrounding neighbors. This can be accomplished by breaking up a two-unit townhouse, triplex, fourplex or multi-family structure into house size building elements, especially where there is a building height transition from adjoining developments, and using the existing separation pattern and orientation of buildings in the

adjacent neighborhood to establish the pattern of the new development. Taller buildings must be stepped down when adjacent to neighboring buildings that are not as tall.

6.6.3 Articulation and modulation of buildings and rooflines reduces perceived building bulk and scale.

6.6.4 Distance from other structures and features will be considered in reviewing scale. Design two-unit townhouse, triplex, fourplex and multi-family buildings to ‘step back’ from abutting lower density properties to better fit into the existing neighborhood.

6.6.5 Building site placement and size should minimize obstruction of views to mountains, Whitefish River, and Whitefish Lake.

6.6.6 Projects with multiple buildings to be built in phases on one site must include design information for each building. Initial buildings must meet all the requirements of the Architectural Review Standards. Information on buildings to be built in the future must include an entire site development narrative with a site plan describing buildings, locations, access for parking and pedestrians, general landscaping and concept of building sizes, shapes, materials and colors.

6.7 Shape and Form

All buildings shall have a shape and form regarding rooflines and massing that relates to the surrounding buildings and neighborhood. Rooflines should be pitched, pitched with false fronts, and shed with false fronts, hipped, gable or gambrel. Overhanging eaves are encouraged.

6.7.1 All remote mechanical and electrical equipment (condensers, chillers, intake/exhaust fans, cabinets, etc.) regardless of horsepower, must be enclosed within sound absorptive walls as permitted by the Building Code. These areas shall be located and screened so the visual and acoustic impacts of these functions are fully contained and out of view. Enclosures shall be reviewed by the Committee where applicable and the acoustic design will be reviewed and approved at the time of plan examination by the Building Department.

6.7.2 Multi-Family Developments

In any development containing more than one two-unit townhouse, triplex, fourplex and/or multi-family structure, the following details are reviewed:

a) Buildings must exhibit a general similarity of scale, orientation, and proportion with unified natural and built features. Buildings must be grouped or sited with other buildings to create distinct outdoor spaces. Sites must connect to natural corridors and trail systems and must facilitate pedestrian and bicycle traffic.

b) As a general rule, taller buildings must be placed toward the interior of the site and stepped back from the street.

c) Buildings must provide visual variety in exterior appearance from other multi-family structures in the same development. The primary objective of this standard is to avoid visual monotony among two-unit townhouse, triplex, fourplex and multi-family structures while encouraging a common high-quality design theme. If two or more developments containing more than one two-unit townhouse, triplex, fourplex and/or multi-family structure occur near one another, the Architectural Review Committee will review building plans for such structures to assure that they are all substantially different from similar two-unit townhouse, triplex, fourplex and multi-family structures in a neighboring development. The intent of these requirements is to create variety and visual interest among all units in a development containing two-unit townhouse, triplex, fourplex and multi-family structures, or located near one another. All design elements can be utilized to create a substantially varied exterior appearance, including but not limited to the following:

- Color (although by itself is not enough to create substantial difference in exterior appearance)
- Texture
- Materials
- Orientation on the site
- Height
- Mass
- Scale
- Shape
- Form
- Façade Modulation
- Roof Articulation
- Articulation of Building Materials and Elements
- Transition Spaces



Garage Behind with Shared Driveway



East Elevation

Garages off Each Street on Corner



Garages to the Side



North Elevation

6.8 Garage Forward Designed Structures

Garage forward designed residential units generally lack connectivity to the street and neighborhood. When streets are more comfortable and inviting places to be, there is more pedestrian activity on the street, which in turn promotes more interaction with neighbors, places more “eyes on the street” and encourages residents to use the front areas of their home and front yards.

The use of garage forward design two-unit townhouse, triplex, fourplex and multi-family structures is not allowed unless the use of garage doors constructed forward of the foundation line of the main structure is unavoidable. If this design is unavoidable, the applicant must employ a cohesive design strategy to mitigate their appearance by using three or more of the following measures:

- Install two single garage doors instead of one two-car garage door;
- Use garage doors that break up the mass of the one long wall – this can be accomplished

by using carriage-style doors, multi-paned doors, or doors with windows;

- The finish color be complementary to the structure;
- Design a shed roof or other architectural feature over the top of the garage doors 2 to 3-feet deep;
- Relief between the two garage doors;
- Moving the entrances so they are easily seen from the street;
- Putting windows and/or living space above the garage;
- Use different techniques for garage doors installed adjacent to one another;
- Incorporate a compatible window in the garage so it appears to be a habitable portion of the house; or
- Any other creative design solution(s) to mitigate the garage forward design accepted by the Architectural Review Committee.



Multi-Family Stepped Down to Existing Single Family Homes

6.9 Exterior Materials

6.9.1 Exterior finish building materials should be of a nature in color and texture complementary with the structures within each building district. They shall not clash, detract or conflict with adjacent architecture.

6.9.2 Acceptable exterior cladding materials should include properly scaled brick, 1x4, 1x6 or 1x8 horizontal natural wood siding, sawn shingles 1x6 or 1x8 vertical channel natural siding, 1x8 to 1x12 vertical board and batten natural wood siding with 1x2 batts, natural stone veneers, and stucco with wood trim. Other fire resistant and ecological friendly materials may be considered.

6.9.3 Wood shingles, vertical wood siding and boards with batts are acceptable in gables and upper portions of walls.

6.9.4 Exposed plain concrete block, and vinyl cladding are not allowed. Metal siding, Exterior Insulation Finish System (EIFS), composite siding and stone panels are discouraged but may be used at the discretion of the Committee. (Exception: residential buildings of two or more units when located in a strictly residential district may utilize composite sidings such as fiber cement if compatible with neighboring structures.)

6.9.5 Acceptable roofing materials shall be asphalt composition shingles, metal, or appropriate flat roofing. Other technologically advanced products such as solar roof tiles are encouraged.

6.9.6 Roofing shall be of colors that complement neighboring structures.

6.9.7 Mirrored and tinted glazing is not allowed. Low-E glass is acceptable.

6.9.8 Exterior classic details and elements such as brackets, cornices, window trim, and wall corner boards are encouraged.

6.9.9 Higher quality and easily maintained materials are strongly recommended.



New Townhome in Existing Neighborhood



New Multi-Family in Existing Neighborhood



6.10 TWO-UNIT TOWNHOUSES/TRIPLEXES

Two-unit townhouses and triplexes are permitted in many zoning districts throughout the City. In addition, depending on the neighborhood, two-unit townhouses and triplexes may be used as infill housing in neighborhoods traditionally developed with single family detached homes. In order to ensure these projects are consistent and compatible with the surrounding neighborhood character and transition effectively to single family detached homes, the following apply:

6.10.1 Ensure buildings are pedestrian friendly and contribute to the character of the surrounding neighborhood. Front porches must be emphasized. If no front porch is provided, the front door must be oriented, so it directly faces the street.

6.10.2 Buildings should be oriented in a way similar with the neighborhood.

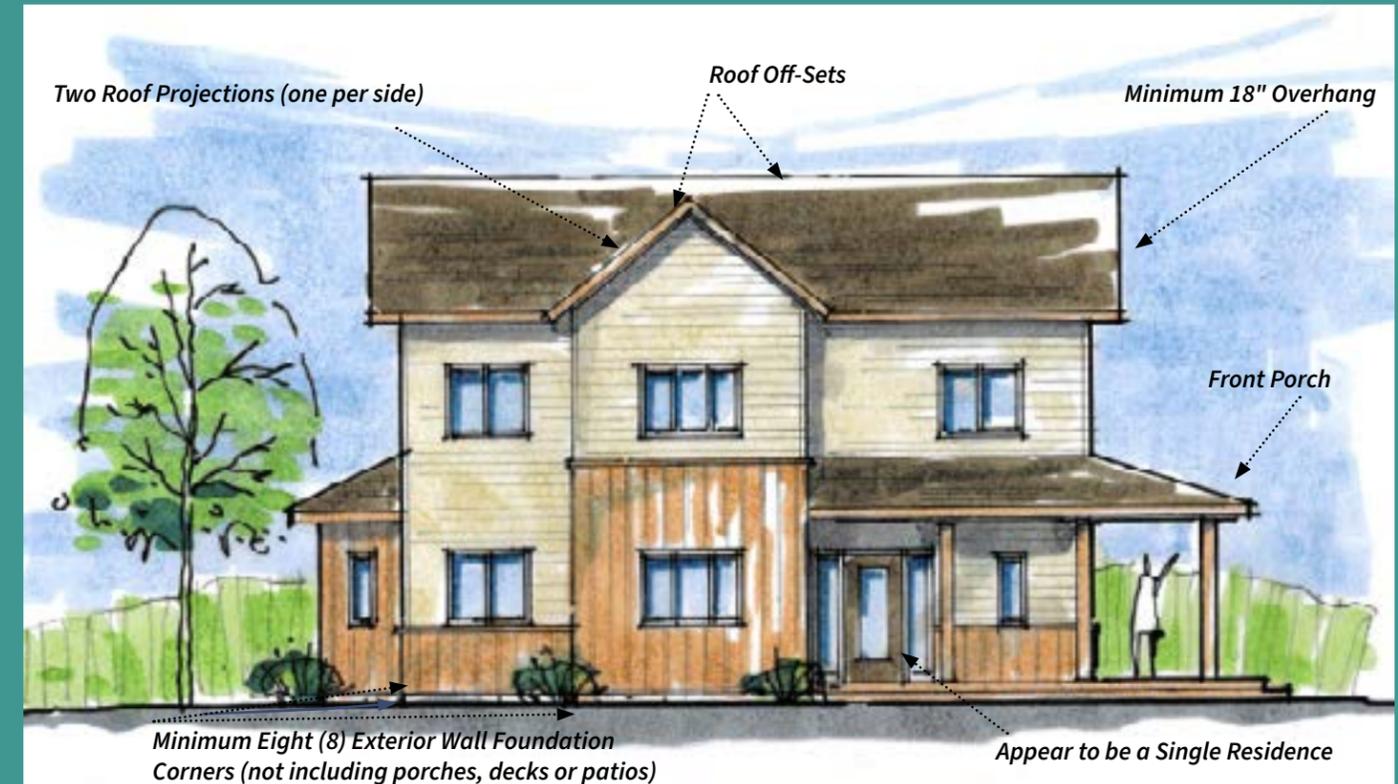
6.10.3 Design of the buildings should reflect architectural styles compatible with the neighborhood and make every attempt to appear like a single-family home.

6.10.4 Structures located on corner lots shall be designed with pedestrian entries located on opposite street frontages, so the structure appears to be a single-family dwelling. Where no alley is available for vehicular access, separate driveways for each unit should be placed on opposite streets.

6.10.5 Meet the minimum requirements identified in the graphic on the next page.

6.10.6 The enhancement features in the graphic may also be included to further support a superior design. NOTE: Not all features are required on a single building.

MINIMUM REQUIREMENTS



ENHANCEMENT FEATURES



PUBLIC AND INSTITUTIONAL FACILITIES

Chapter 7

Public and Institutional Facilities are located throughout the various design districts. These facilities include administrative offices, maintenance buildings, bus barns, schools, park restrooms, park facilities and community buildings. Some of these facilities are designed to be central features of our community, such as a city hall, performing arts center or library. Other structures are not intended for general public use, such as a maintenance facility. Facilities developed with public funds are designed for low-maintenance and long-term use, but still need to incorporate elements from the design district where they are located.



Public and Institutional Facilities will be designed to honor the neighborhood where they are located, relate to the street and surroundings and meet the needs of the community.



Photo Credit: Whitefish Pilot

GLOSSARY OF ARCHITECTURAL TERMS

- **Articulation:** Shifts in the plane of the walls, setbacks, stepbacks, reveals, overhangs and details in order to create variations in the building's façade
- **Building Elevations:** A one-dimensional graphic representation of a proposed building
- **Cornice:** Any horizontal decorative molding which crowns any building
- **Fenestration:** Products that fill openings in a building, such as windows, doors, skylights, curtain walls, etc., designed to permit the passage of air, light, vehicles, or people
- **Form:** The shape, appearance, or configuration, of a building
- **Formula Retail:** A type of retail sales activity or retail sales establishment, including restaurant which, along with twelve or more other retail sales establishments, maintains two or more of the following features: a standardized array of merchandise, a standardized façade, a standardized décor and color scheme, uniform apparel, standardized signage, a trademark or service mark. See the full definition in §11-9-2, WCC.
- **Full Cut-Off Lighting:** A luminaire designed and installed where no light is emitted at or above a horizontal plane running through the lowest point on the luminaire
- **Gable Roof:** The vertical triangular portion of the end of a building with a two-way pitched roof, extending from the level of the cornice or eaves to the ridge of the roof
- **Gambrel Roof:** A type of gable roof where the end of the building is hipped
- **Glazing (mirrored, tinted, Low-E):** A transparent part of a wall usually made of glass
- **Hipped Roof:** A type of roof where all sides slope downwards to the walls, usually with a gentle slope
- **Lintel:** A horizontal feature (such as a beam) over an opening which carries the weight of the wall above it
- **Mansard Roof:** A type of hip roof characterized by two slopes on each of its four sides with the lower slope being much steeper, almost a vertical wall, while the upper slope, usually not visible from the ground, is pitched at the minimum needed to shed water
- **Mass:** A building's bulk, size and magnitude – the overall volume
- **Mechanical Equipment:** Includes venting, heating or cooling equipment used for buildings
- **Modulation:** Variations of a building's mass using deep setbacks, diminishing upper floor areas and/or projecting roof overhangs
- **Neighborhood Context:** How a project blends into and integrates with the context of the adjacent neighborhood and uses
- **Pedestrian Spaces:** Any open area designed to be used for pedestrians, including, but not limited to a plaza, walkway, etc.
- **Perspective Drawing:** A three-dimensional graphical representation of a proposed building
- **Relief, Architectural:** A change in the setback (forward or back) to a vertical façade
- **Roof Pitch:** A roof's pitch is the measured vertical rise divided by the measured horizontal span
- **Scale:** The spatial relationship of a building with neighboring buildings
- **Shed Roof:** Single pitched roof
- **Sky Glow:** A kind of light pollution visible by the glowing effect seen in the skies over many cities and towns
- **Sonotubes:** A concrete form used for a foundation